

FULLERTON TRANSIT VILLAGE

Calvary Chapel
OF FULLERTON

Specific Plan
Administrative Draft #3

Prepared for
City of Fullerton
The Olson Company

Prepared by
William Hezmalhalch Architects Inc.

May 10, 2004

City Council Meeting
May 18, 2004
Agenda Item #2

**FULLERTON TRANSIT VILLAGE
SPECIFIC PLAN
ADMINISTRATIVE DRAFT #3**

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SECTION ONE

Executive Summary

- 1.1 Purpose and Objectives
- 1.2 Authority for Specific Plans
- 1.3 Relationship to CEQA
- 1.4 Project Location
- 1.5 Project Overview
- 1.6 Specific Plan Document Organization

1.0 EXECUTIVE SUMMARY

1.1 Purpose and Objectives

The Fullerton Transit Village Specific Plan document is intended to provide for the orderly and efficient development of the Specific Plan area in accordance with the provisions of the City of Fullerton General Plan. It contains development standards and design guidelines which apply only to property within the Specific Plan area that are intended to implement the goals, objectives and policies of the City's General Plan. In developing the Fullerton Transit Village Specific Plan, the land development concept has been designed with the following goals in mind:

- To implement the General Plan policies by presenting more detailed direction for future development.
- To reclaim the currently blighted site and transform it into an aesthetic living environment, integral with downtown Fullerton.

- To establish zoning standards and implementation mechanisms applicable solely to the Fullerton Transit Village project.
- To provide for multi-family housing and live/work opportunities responsive to local needs and transit opportunities.
- To provide a desirable community where people will want to live.
- To establish a compatible interface with adjacent land uses.

Implementation of the Fullerton Transit Village Specific Plan provides the parameters for establishment of a cohesive urban infill, planned development. This will be achieved by coordinating the land use, intensity, scale and aesthetic characteristics of development with the goals and policies of the Redevelopment Area and General Plan.

1.2 Authority for Specific Plans

The Fullerton Transit Village Specific Plan is prepared and established under the authority granted to the City of Fullerton in accordance with the requirements of the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450 and 65457. The California Government Code authorizes cities to adopt specific plans by resolution or by ordinance. A public hearing is required, after which the Specific Plan must be adopted by the Fullerton City Council for final approval.

The Fullerton Transit Village Specific Plan is intended to be a regulatory document which will serve as the zoning regulation for the property. Some elements of the development program will be enforced through conditions, covenants and restrictions (CC&R's) established in conjunction with the subdivision map for the property. These include:

- The number of public parking spaces and hours of allowed use
- The city of Fullerton as third party

- Parking in designated spaces only.. Parking in roadways prohibited
- Emergency Evacuation Access to be marked for Fire Dept. access only and maintained clear at all times

1.3 Relationship to CEQA

Prior to the Olson Company involvement with the subject site, The Fullerton Town Terrace EIR (S.C.H.# 88081712) was prepared for a project area including the Fullerton Transit Village site. This EIR certified the HDR General Plan designation and R-5 Zone classification, and was based on a more dense land use than that being proposed by The Olson Company (the EIR certified 346 apartments in 5, 4 and 3 story buildings and a 2 story parking garage). New traffic, noise and sewer studies have been prepared for the new application to verify impacts and no change in environmental conditions or new impacts have been identified as a result of the subject project. The City of Fullerton has determined that the Fullerton Transit Village Specific Plan proposal is consistent with the previous EIR, and no new environmental

documentation will be required, pursuant to CEQA Section 15153.

1.4 Project Location

The project site is located within the City of Fullerton in Orange County, in southern California, as shown in Exhibit 1, Regional Location Map, and Exhibit 2, Local Vicinity Map. The City of Fullerton is generally located 22 miles southeast of Los Angeles in North Orange County.

The 5.4 acre Specific Plan site is within the central portion of Fullerton, south of the downtown area. The site is bordered on the south by East Truslow Avenue and existing residential housing; on the west by Harbor Blvd. and the historic “Ice House” building; on the north by the Walnut Avenue, the railroad tracks, and a Southern California Edison sub-station; and on the east by Walnut Way and the electrical sub-station as shown on Exhibit 4, Surrounding Land Use. Access to the site from the surrounding region is by the 91 Freeway via Harbor Blvd. and the 57 Freeway via Orangethorpe Avenue.

1.5 Project Overview

The Fullerton Transit Village Specific Plan land development concept would transform the currently blighted and vacant project site into a planned residential community with approximately 120 dwelling units on 5.4 acres. The product mix includes 66 loft units (including up to 19 live/work units) and 54 townhome units (including up to 8 live/work units). The residential product mix consists entirely of multi-family dwelling units arranged in alley-loaded cluster configurations. The cluster-style plotting reduces the appearance of repetitive massing from external viewpoints. The proposed 120 dwelling units are an average of 1,560 square feet in size.

1.6 Specific Plan Document Organization

The Fullerton Transit Village Specific Plan defines a vision and establishes guidelines for the development of the site. The basic approach for preparing this Specific Plan has been to recognize the interrelationship between land use, design, regulation, and sound economic, market, and financial

considerations. The Specific Plan is arranged into seven sections as follows:

Section 1.0 Executive Summary

This chapter includes a discussion of the intent and purpose of the Specific Plan, the authority and scope, a description of location of the site, the objectives of the project, and an overview of the project.

Section 2.0 Planning Context

This chapter provides a summary of the existing conditions affecting the Specific Plan area, including existing City land use designations, existing and surrounding land use discussion, and a summary of the existing infrastructure within the project area.

Section 3.0 Specific Plan Elements

Forming the core of the Specific Plan document, this chapter presents the Land Use Plan for the project, including proposed

land use designations, and discusses the proposed circulation, grading, and infrastructure of the Land Use plan.

Section 4.0 Development Standards

This chapter specifies the permitted and conditionally permitted uses in the proposed land use designation, and specifies standards for development, such as lot coverage, building height, setbacks, etc.

Section 5.0 Design Guidelines

A creative yet flexible set of guidelines and design criteria for site planning, architecture, and landscaping is specified, providing direction for streetscape improvements, project identification, hardscape elements and architectural guidelines.


Section 6.0 Implementation

This chapter contains provisions for financing/maintenance of improvements, a process for implementation of this Specific Plan and procedures for amending this Specific Plan.



Section 7.0 Consistency with Fullerton General Plan

Chapter 7.0 discusses how the Specific Plan conforms with applicable City of Fullerton General Plan goals and objectives.



SECTION TWO

Planning Context

- 2.1 Jurisdictional Considerations
 - 2.1.1 General Plan
 - 2.1.2 Zoning Ordinance
 - 2.1.3 Redevelopment
 - 2.1.4 Fullerton Transportation Center Study
- 2.2 Existing Conditions
 - 2.2.1 Land Uses on Site
 - 2.2.2 Surrounding Land Uses and Relationship
 - 2.2.3 Traffic and Circulation
 - 2.2.4 Aesthetics
 - 2.2.5 Public Utilities and Facilities

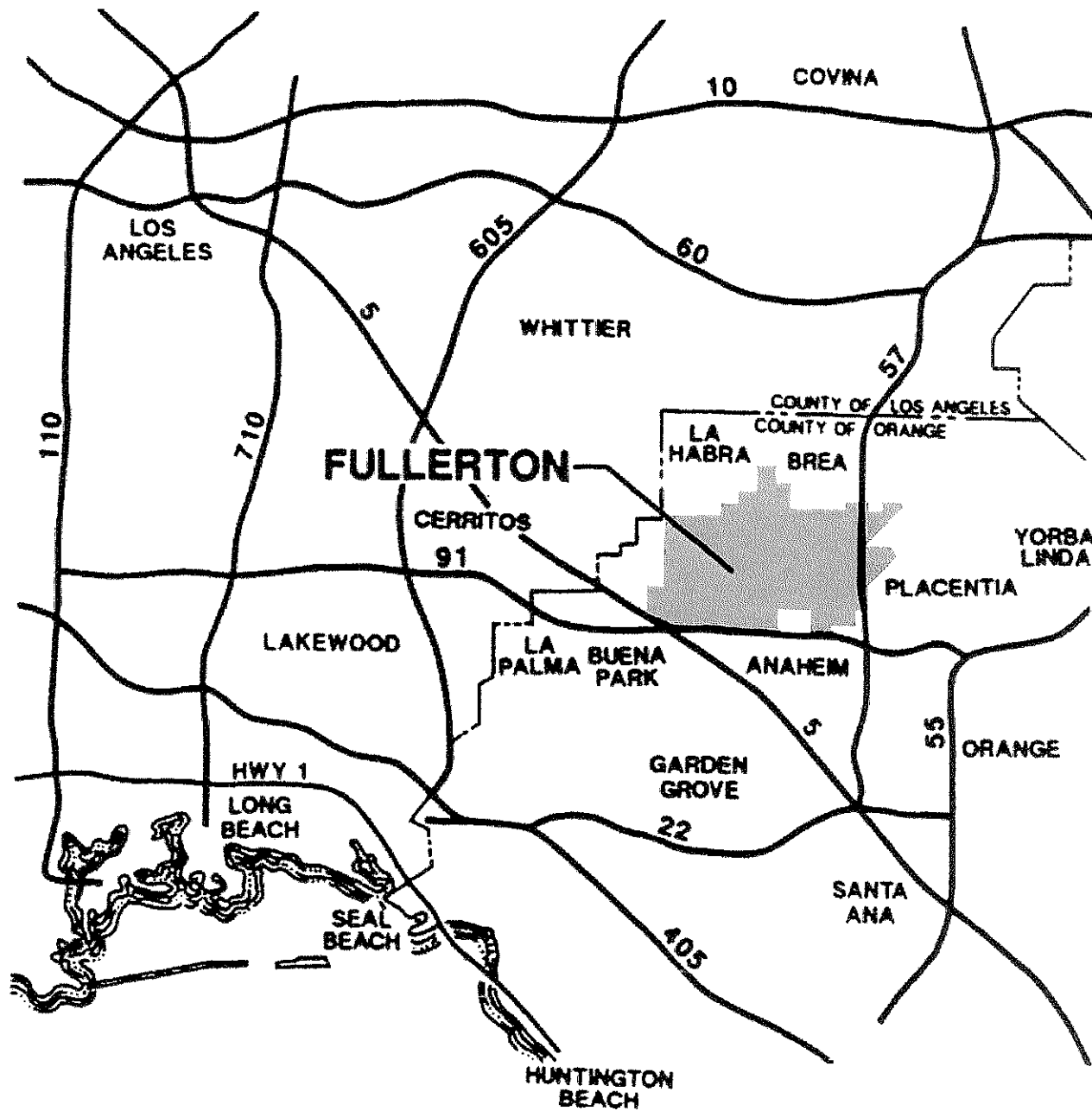


EXHIBIT 1
Regional Location Map

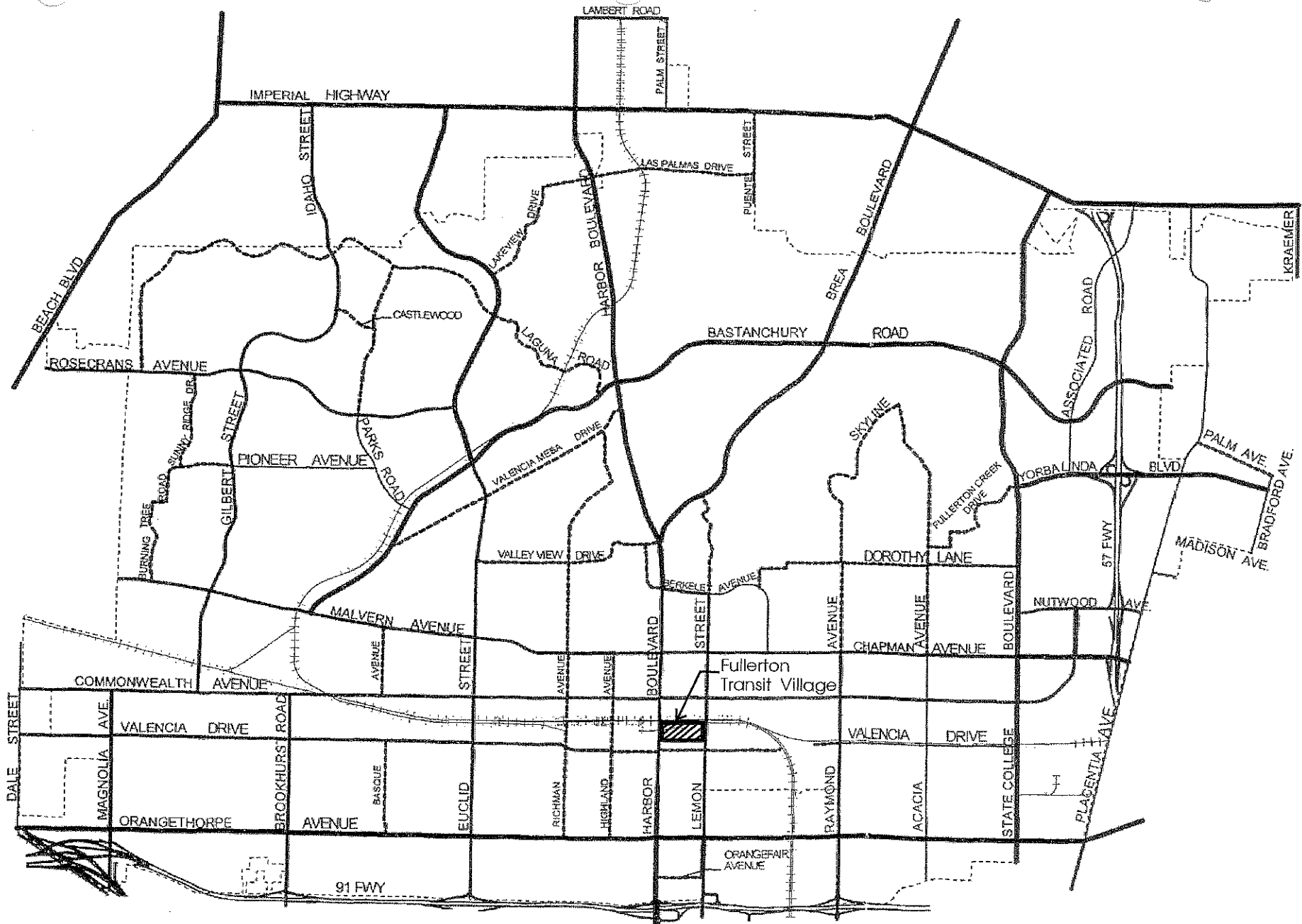


EXHIBIT 2
Local Vicinity Map

2.0 PLANNING CONTEXT

2.1 Jurisdictional Considerations

2.1.1 *General Plan*

The City of Fullerton General Plan designation for the entire site is High Density Residential. This designation is intended to accommodate multiple unit, attached residential densities of 28/du ac. and higher. The project proposes a density of 22.4 du/ac, which would be included in the General Plan designation of Medium Density Residential. The Medium Density Residential designation allows multiple units, attached residential densities from 15 du/ac to 28 du/ac. No General Plan Amendment is necessary to accommodate the proposed application, since Specific Plan Districts (SPD) zoning and associated text are permitted within the High Density Residential designation, pursuant to General Plan Table LU-4. Upon approval, the Fullerton Transit Village project site will be subject to the design guidelines and development standards contained in this document as adopted or amended.

2.1.2 *Zoning Ordinance*

Prior to the adoption of this Specific Plan document, the site area was zoned R-5 (Maximum Density Multiple Family) and M-G (General Industrial) by the City's Zoning Code. The R-5 zone allows for apartments and condominiums close to the Central Business District, and M-G permits general industrial uses. A rezoning from the R-5 and M-G to Specific Plan District was required to implement the project. The R-5 Zone is not compatible with the projects building setbacks, massing, and window treatments. The M-G Zone would not allow multi-family dwellings. The Specific Plan District (SPD) designation allows multi family residential units at an overall density of 22.2 units per acre.

2.1.3 *Redevelopment*

The Specific Plan Study area is included in the Central Fullerton Redevelopment Project Area 2. Specifically, the site is within Sub-Area G, originally designated as industrial use, subject to Agency encouragement in upgrading existing properties.

2.1.4 Fullerton Transportation Center Study

In 2003, the City of Fullerton, in conjunction with their General Plan Update, identified new levels of accessibility provided at the Fullerton Transportation Center as a major opportunity for the city. The Study generated new ideas, plans and guidelines to upgrade areas adjacent to the Transportation Center and to link this area with downtown.

The Fullerton Transit Village site is included in the Southeast quadrant of the overall Study Area. Conceptual uses proposed of the Southeast quadrant include higher density residential (up to 70 du/ac), convenience retail, physical linkage to the Transportation Center, a visual/physical barrier between new residential and the existing electrical transformers, and a residential edge with maximum 2.5 stories along Truslow Avenue.

2.2 Existing Conditions

Existing physical site conditions on and directly adjacent to the subject site are illustrated in site photographs.

Refer to Exhibit 3, Existing Conditions.

2.2.1 Land Uses on Site

Current Land Uses on site include vacant property and vacant single-family and multi-family residences.

Refer to Exhibit 4, Surrounding Land Use.



Fullerton Train Depot



Walnut Avenue Parking



Site From Overpass (looking South)

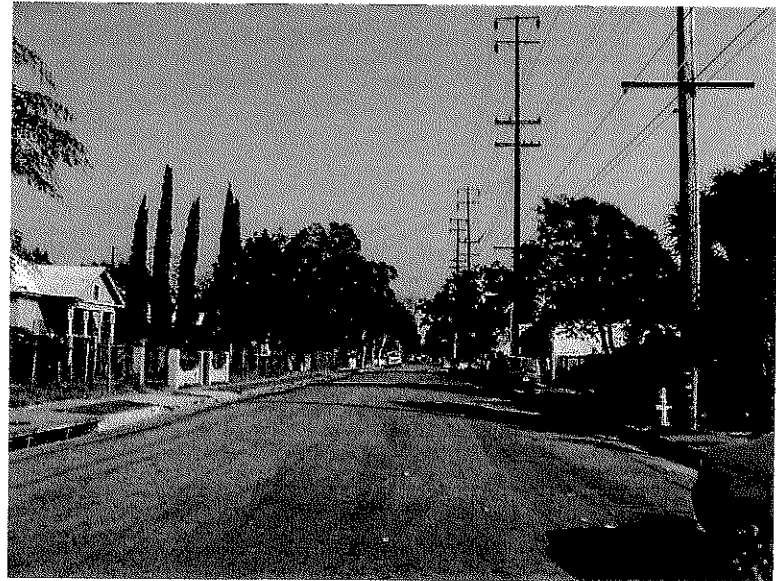


Sub-Station and Walnut Way

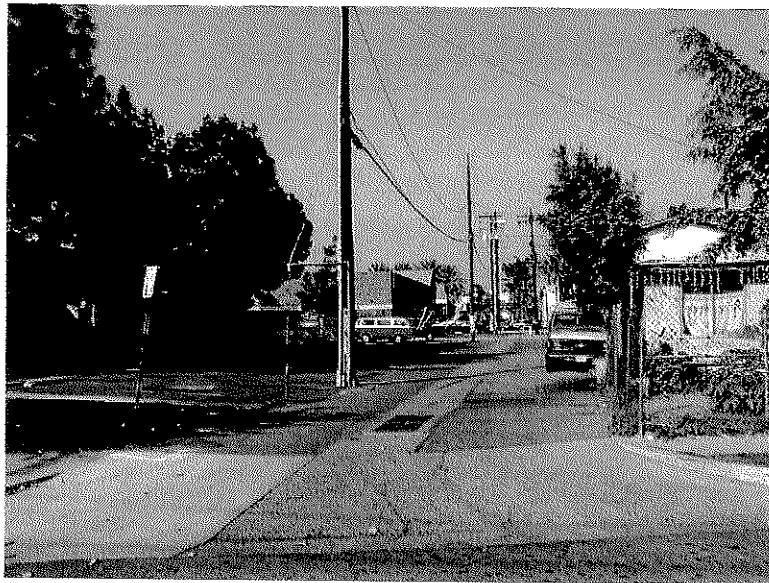
EXHIBIT 3a
Existing Conditions



Walnut Way



Truslow Avenue (looking East)



Truslow Avenue (looking into site)



Harbor Blvd. Overpass

EXHIBIT 3b
Existing Conditions

2.2.2 Surrounding Land Uses and Relationship

The Fullerton Transit Village Specific Plan area is surrounded by the following land uses, as depicted on Exhibit 4, Surrounding Land Uses:

- North: Fullerton Transportation Center, Walnut Avenue
- South: East Truslow Avenue, existing single family and multi-family residential, with minor commercial
- East: Walnut Way, an electrical sub-station, and auto repair
- West: Harbor Blvd., Calvary Chapel (“Ice House”), and an office building

2.2.3 Traffic and Circulation

As discussed above, the project is adjacent to or bounded by East Truslow Avenue to the south, Harbor Blvd. to the west, Walnut Avenue to the north and Walnut Way to the east. General access to the project via Walnut Avenue and Walnut Way are not proposed due to existing congestion at Lemon Street. Primary and secondary access to the Specific Plan area

will be provided from East Truslow Avenue. Additional emergency access will be provided from Walnut Way and Walnut Avenue. A new traffic study was completed for the Specific Plan program by LSA in 2003. The study concluded that the proposed project will contribute impacts to two intersections, Lemon Street/Walnut Avenue and Harbor Blvd./Commonwealth Avenue. Mitigation measures were not recommended for the Lemon St./Walnut Way condition. The cumulative impacts to the Harbor Blvd./Commonwealth Avenue intersection have been foreseen thru the General Plan program, which provides mitigation. Project related mitigation is recommended to be participation in the Intersection Improvement Plan process.

2.2.4 Aesthetics

The Specific Plan area contains a significant amount of vacant and underutilized properties that have been dilapidated for years. The City of Fullerton considered the site to be a visually blighted area suitable for redevelopment with residential uses.

The Specific Plan's architectural design intent and landscape treatment represent significant visual quality/aesthetic enhancement opportunities for the area.

2.2.5 Public Utilities and Facilities

Existing utilities are located in the perimeter streets around the project site. The sections following discuss these different utilities and anticipated upgrades that may be required for the project.

2.2.5.1 Water

Per the City of Fullerton water atlas, a 6 inch diameter cast iron domestic water line is located in Walnut Avenue, Truslow Avenue, Walnut Way, and in the existing 20 foot wide alley along the westerly side of the project that parallels Harbor Blvd.

The project will be served with a new 8 inch main that will run through the site. In addition, the 6 inch main in Truslow will be replaced with an 8 inch main from Harbor Blvd. To Walnut

Way. A new 8 inch main will be installed in Walnut Way from Truslow to "B" Street within the new project. A new 8 inch line will be installed in Walnut Avenue that will connect to the existing 8 inch main in Walnut Avenue west of Harbor Blvd. and travel easterly to tie into a point of connection on the north side of the project. A 10 inch or 12 inch main will also be built from the new main at Truslow and Walnut Way to the existing 16 inch main in Lemon Street. Additional studies will be needed to determine the final design of the system.

2.2.5.2 Sewer

Sewer lines currently exist in East Truslow (6 inch VCP) and (12 inch VCP) East Walnut Avenue (8 inch VCP).

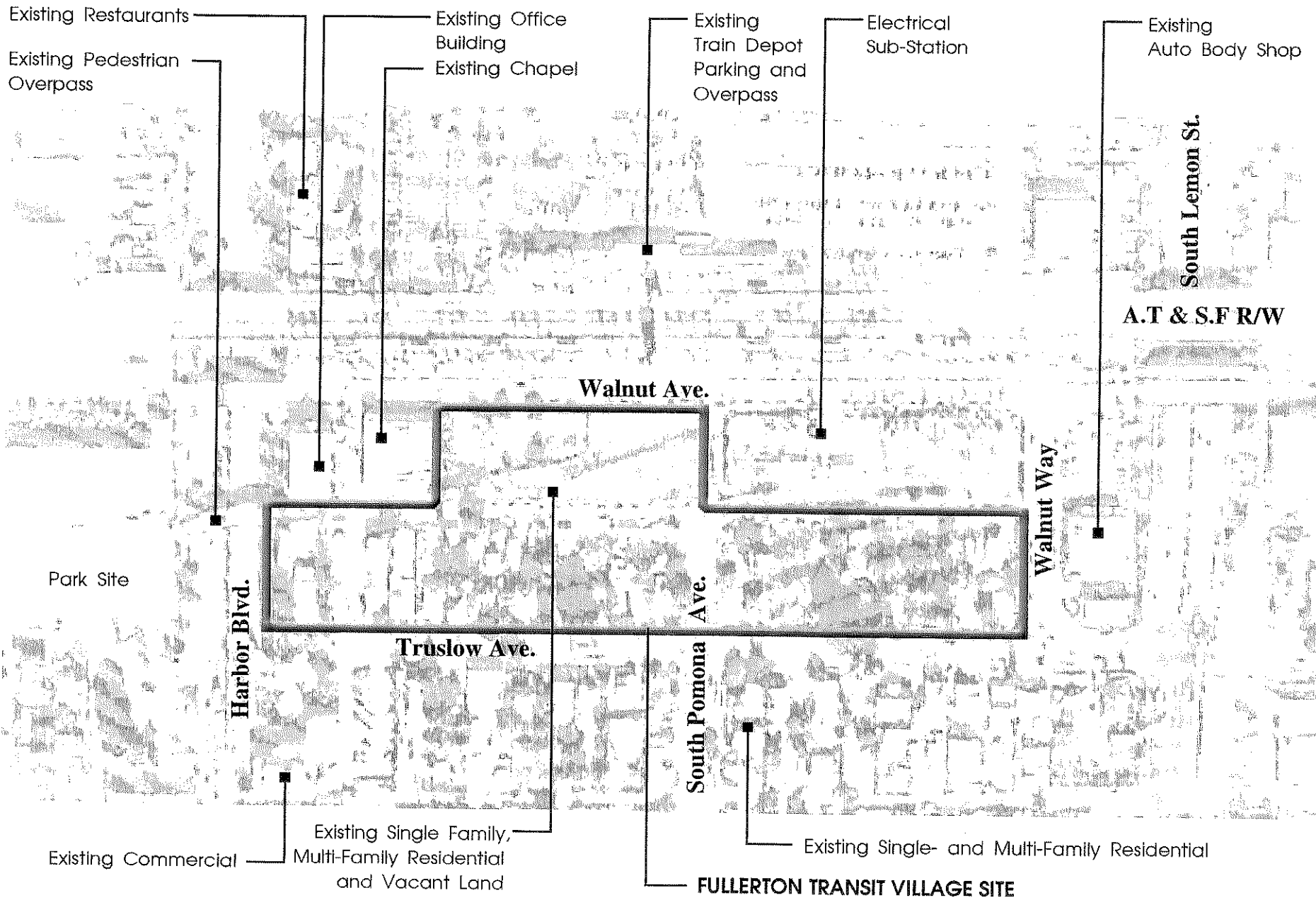


EXHIBIT 4
 Surrounding Land Use

2.2.5.2 Drainage

The existing area generally drains from north to south. Per the “City of Fullerton Existing Drainage Facilities” Map, there exists a storm drain ranging in size from 36 inch to 42 inch diameter reinforced concrete pipe in the westerly 20 foot alley. Two 14 foot catch basins in East Truslow Avenue are connected to the main line via 18 inch diameter pipes. An existing parkway culvert conveys drainage under East Walnut Avenue and outlets into existing Pomona Avenue. Drainage flows south into Fullerton Creek Channel.

2.3 Opportunities and Constraints

The Land Use Plan presented in Chapter 3, Specific Plan Elements, which follows, was designed to incorporate various existing physical site opportunities and constraints, if feasible. These included:

Opportunities

- Adjacency to the Fullerton Transportation Center
- Live/Work potential

- “Walkability” to Downtown Fullerton
- Pedestrian connection over Harbor Blvd. to the public park
- Direct pedestrian access to train station via overpass

Constraints

- Electrical transformers (66kv)
- Existing zoning
- Adjacent residential conditions
- Noise from trains
- Heavy traffic along Harbor Blvd. (noise)
- Adjacent autobody center along Walnut Way



SECTION **THREE**

Specific Plan Elements

- 3.1 Land Use plan
- 3.2 Circulation
 - 3.2.1 Project Circulation / Access
 - 3.2.2 Parking Considerations
- 3.3 Conceptual Grading Plan
- 3.4 Conceptual Infrastructure Plan
 - 3.4.1 Water
 - 3.4.2 Sewer
 - 3.4.3 Drainage
 - 3.4.4 Utilities

3.0 SPECIFIC PLAN ELEMENTS

3.1 Land Use Plan

The proposed new community for the Fullerton Transit Village Specific Plan will represent a significant change from the site's current vacant single family and multi-family residential uses. The Land Use Concept, shown in Exhibit 5, provides an overall vision and guide for the ultimate development of the site. The design approach takes advantage of the site's location adjacent to the Amtrak/Metrolink Station to the north to create a transit village and integrates it into the downtown setting.

The plan proposes to create a blend of residential product types in a pedestrian-friendly environment. Live/work dwelling units are proposed in key locations throughout the project, including the north/south pedestrian corridor anchored by the pedestrian overpass landing (over Walnut Avenue), and along the Walnut Avenue. There is also the potential for live/work opportunities along the Truslow Avenue and Walnut Way frontages, and facing the historic "Ice House". The Specific Plan allows a

maximum of 120 multi-family units at an overall density of 22.2 units per acre, consisting of 66 live/work loft units, (6 & 8 plex buildings) 12 live/work townhome units in 6-plex buildings, and 42 traditional townhome units in 4-plex and 6-plex buildings. Live/work unit locations are defined on Exhibit 5, Land Use concept. The initial number of live/work units permitted in the project is 23. However, additional live/work units can be permitted based on an availability of public access and parking, and compliance with the building code.

Table 1, Proposed Land Use Summary, presents a statistical summary of proposed development. It is the intent of the Specific Plan to permit a unique product type and urban development that would otherwise not be permitted by the City's standard zoning requirements. It is also the intent of the Specific Plan to provide flexibility in terms of the product type, actual location and acreage of the residential product, so that products can be developed and rearranged within the residential planning area as long as the overall unit product densities are not exceeded.

TABLE 1 Proposed Land use Summary				
Land Use Description	Total Dwelling Units	Max. Dwelling Units	Net Acres	Net Density
Multi-Family Lofts	66	66	1.16 ac (buildings)	23.8 du/ac
Multi-Family Townhomes	54	54	1.07 ac (buildings)	20.3 du/ac
Landscaped Open Space	-	-	1.2 ac	
Roads	-	-	1.71 ac	
Parking	-	-	.23 ac	
Driveways	-	-	.03 ac	
Total	120	120¹	5.4 ac	22 du/ac²

¹The maximum d.u. for each product type is 120

²The maximum allowable density is 30 du/ac

The clustered homes will be a mix of 2 products with floor plans ranging from 1200 square feet to 1800 square feet.

All plans will have a 2-car garage, up to 3 bedrooms and 2.5 baths, and three plans will also feature a fourth bedroom or home office. A typical 6 unit building measures 124 foot by 39 foot. The homes will be typically clustered around an alley, which functions as the access drive to the homes. Each loft

and townhome will feature private open space within a patio, as outlined in Chapter 4, Development Standards, except for live/work lofts, which will include a balcony. A key goal of the live/work lofts is to encourage pedestrian/retail traffic, and inclusion of a patio would impede access.

3.2 Circulation

3.2.1 Project Circulation / Access

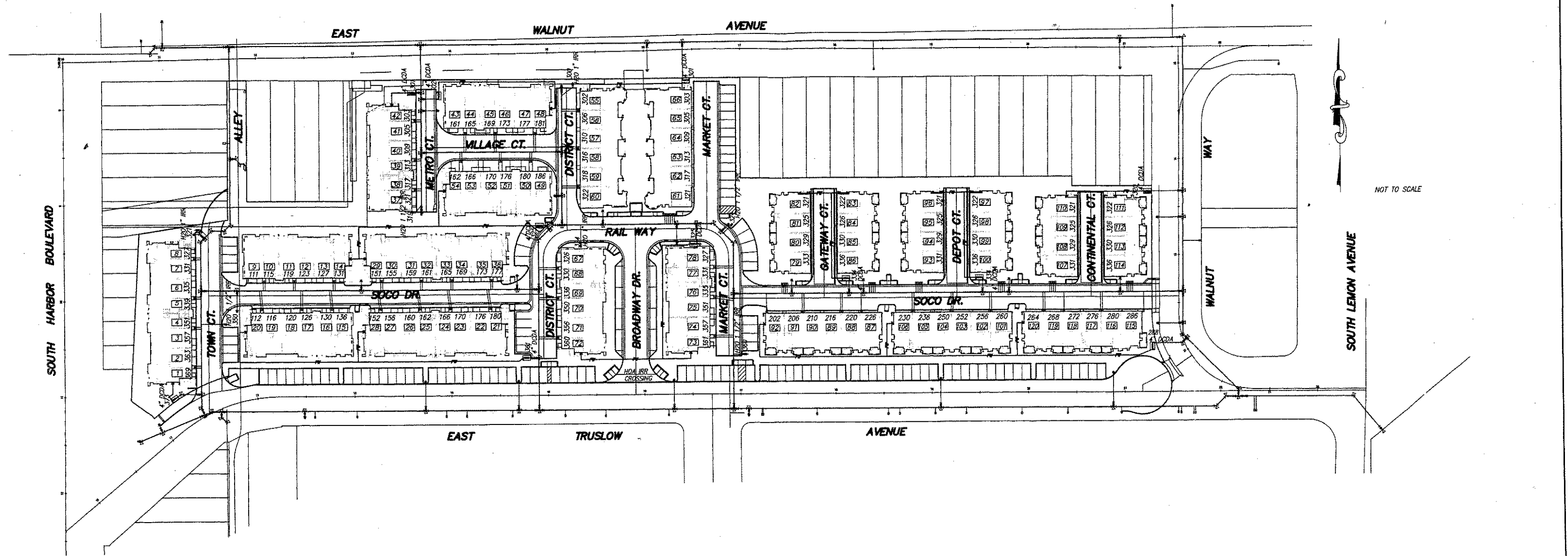
The circulation concept for the site consists of a system of private streets to provide access to the residential units. The project includes a primary entry from East Truslow Avenue and a secondary access to the west on East Truslow Avenue, as shown in Exhibit 6, Circulation Plan. Emergency access is provided from Walnut Way. The primary entrance to the project will feature one 26 foot lane with a special landscape treatment.

The proposed internal entry road will have a 44 foot wide pavement width (consisting of 13-foot wide travelways in each direction and 9-foot wide parking stalls on both sides of the



street), and a 11-foot parkway (including a 5 foot sidewalk and 7 foot landscape strip) on both sides of the street. The proposed on-site circulation system will create a 26 foot wide access road and series of internal alleys.

A future landing is proposed directly across from the existing pedestrian overpass over Walnut Avenue and the train tracks. The landing will include a courtyard concept, with landscape features, all intended as an entry to the live/work units directly adjacent. Access to this future landing will be provided to the general public from Truslow Avenue, along the primary entry and through the live/work unit courtyard.

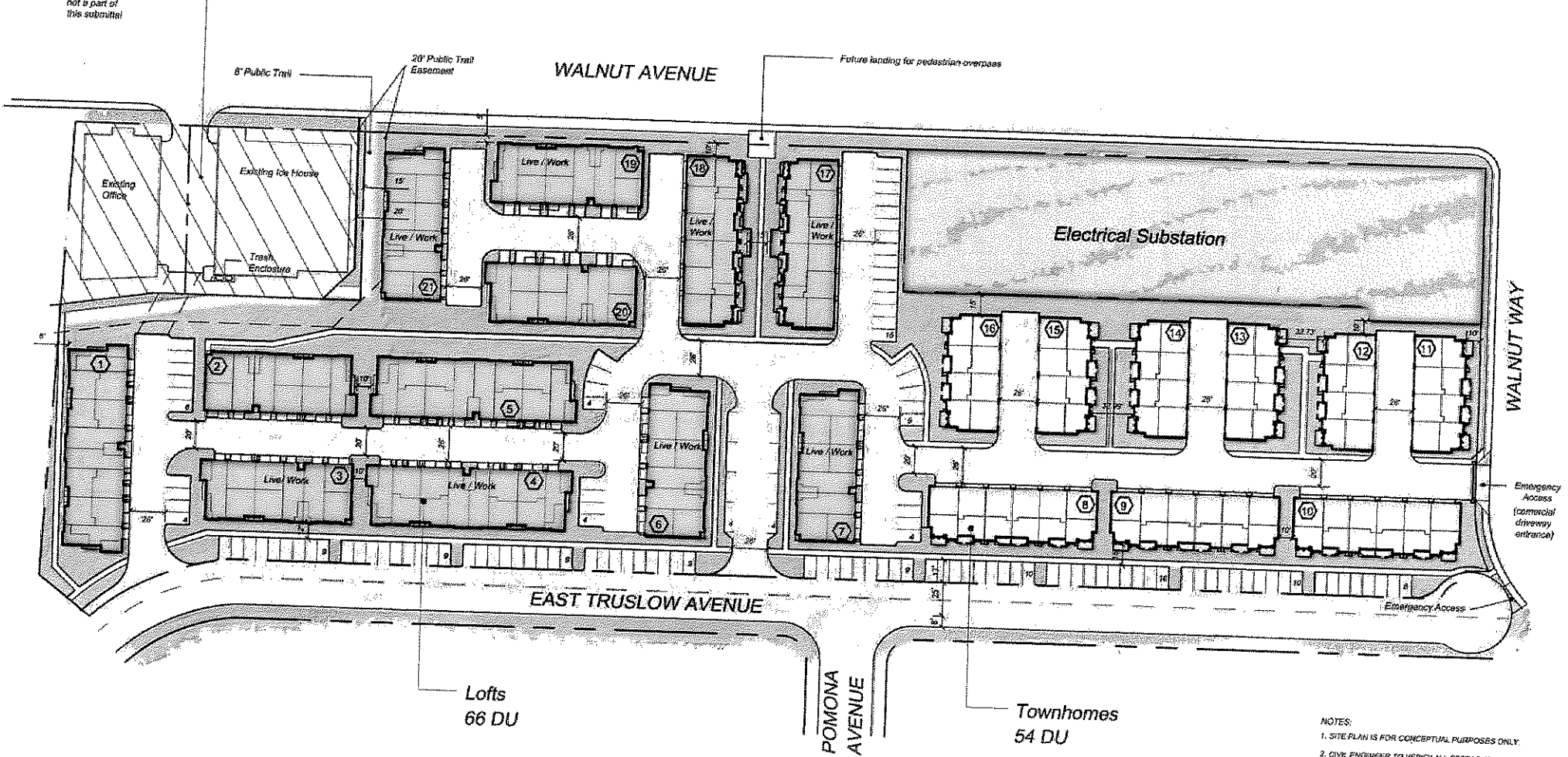


REVISIONS						REFERENCES						CORROSION INDEX		CITY OF FULLERTON			
NO.	INITIAL	DESCRIPTION	APPROVED BY	DATE	FIELD BOOK	PAGE	BENCH MARK NO.	ELEVATION	FOR STANDARD DETAIL SEE	CORROSION INDEX	INITIAL	DATE	OFFICE OF THE CITY ENGINEER WATER SYSTEM MANAGEMENT DIVISION TRACT 16583 WATER PLAN AND ADDRESS MAP				
										1							DATE
											25			SCALE: AS SHOWN	FILE NO.	SHEET	OF SHEETS

THESE PLANS WERE PREPARED UNDER THE SUPERVISION OF:

 DATE _____ REGISTERED CIVIL ENGINEER NO. _____

NOT TO SCALE



SITE SUMMARY:

SITE AREA:	5.40 ACRES
TOTAL UNITS:	120 UNITS
DENSITY:	22.2 UNITS/ACRE
PARKING PROVIDED:	
GARAGES	240 SPACES
OPEN PARKING	50 SPACES
TRUSLOW PARKING	83 SPACES
TOTAL	373 SPACES
PARKING RATIO	3.1:1

NOTES:
 1. SITE PLAN IS FOR CONCEPTUAL PURPOSES ONLY.
 2. CIVIL ENGINEER TO VERIFY ALL SETBACKS AND GRADING INFORMATION



EXHIBIT 5
 Land Use Concept

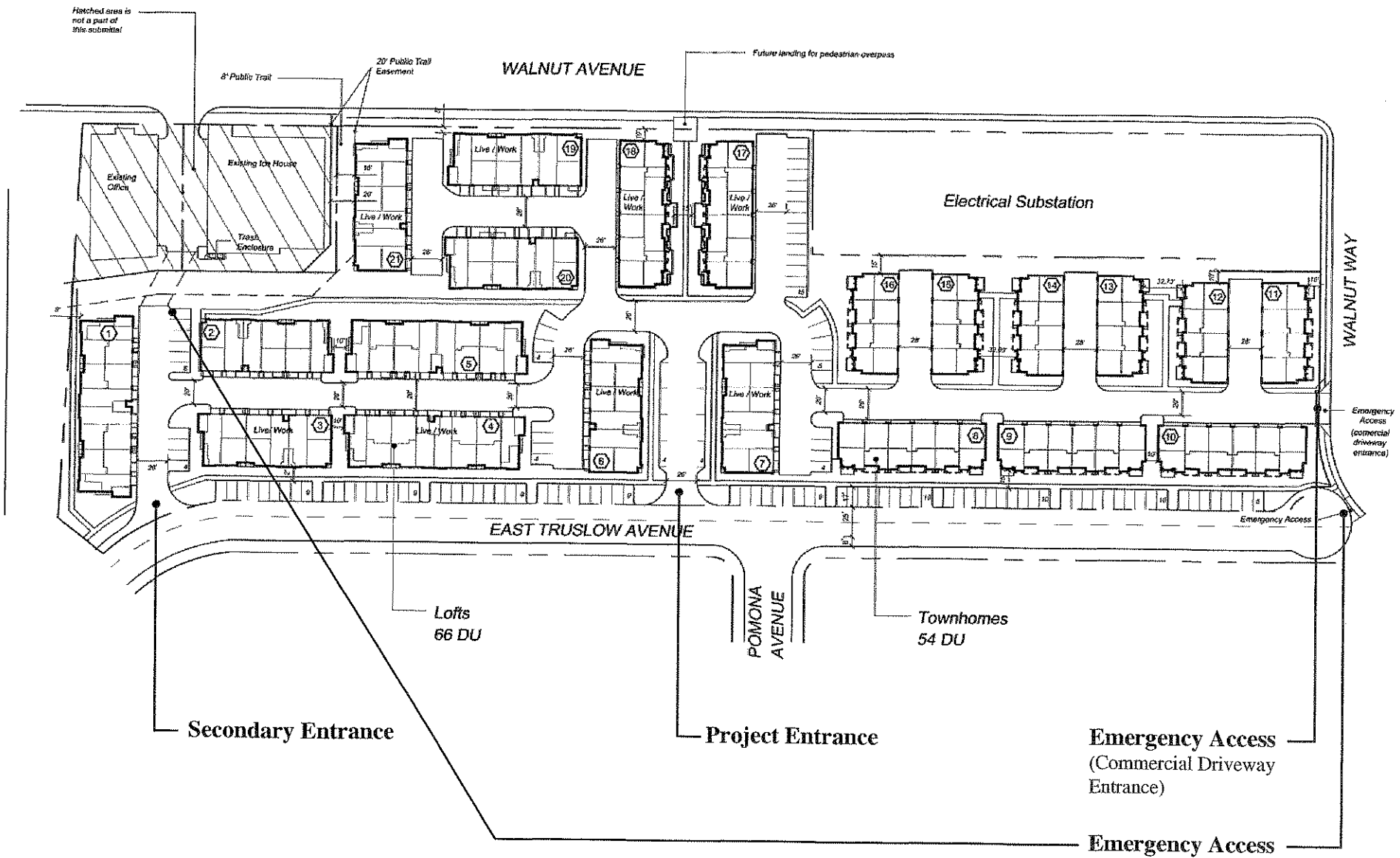


EXHIBIT 6
 Conceptual Circulation Plan

A 20 foot wide public trail easement will connect the park site west of Harbor Blvd., via the existing pedestrian overpass, with the Fullerton Transit Village site and Walnut Avenue. The easement will include a 10 foot wide concrete path and 10 feet of landscaping. Trail access will be provided to the church and proposed loft units. All trail and landscaping improvements will be made by the developer.

3.2.2 Parking Considerations

A total of 133 guest parking spaces will be provided within the Specific Plan area and along East Truslow Avenue, including 8 parallel and 42 head-in guest stalls on-site and 83 head-in guest stalls along East Truslow Avenue. Overall, this total equates to approximately .5 guest parking space per dwelling unit within the development “commons” and directly adjacent to the homes. Refer to Exhibit 6a, Parking Plan, for the approximate location of all street parking spaces.

The 83 head in stalls included on the project site, directly off Truslow Avenue, are to be preserved for public use during*the weekdays. At minimum, public parking shall be permitted

between 6 a.m. and 7 p.m. Additional restrictions may be established by the developments CC&R’s. The intent is to assure some level of accommodation for Transit Center users who are displaced by the projects street abandonments, and parking for business clients of the live/work units.

3.3 Conceptual Grading Plan

A preliminary grading plan that accommodates the site plan requirements of the Fullerton Transit Village project is shown on Exhibit 7, Conceptual Grading Plan. The site is relatively flat, precluding the need for extensive grading. The grading concept along the site’s borders is consistent with the existing neighborhood grades to ensure compatibility with the surrounding neighborhoods. Refer to Exhibit 7, Conceptual Grading Plan.

3.4 Conceptual Infrastructure Plan

3.4.1 Water

Existing 6 inch water lines exist around the perimeter of the project. Proposed water lines will be constructed in locations

depicted on Exhibit 8, Conceptual Water Plan, to service the Fullerton Transit Village community.

3.4.2 Sewer

An existing 8 inch sewer line is located in Walnut Avenue. In Truslow Avenue, the sewer line varies from a 6 inch to 8 inch VCP line. A 12 inch VCP sewer line also exists in Truslow Avenue between Walnut Way and Pomona Avenue. This 12 inch line continues south on Pomona Avenue. Proposed sewer lines will be constructed in locations depicted on Exhibit 9, Conceptual Sewer Plan, to service the Fullerton Transit Village community. Additional studies will be needed to determine the final design of the system.

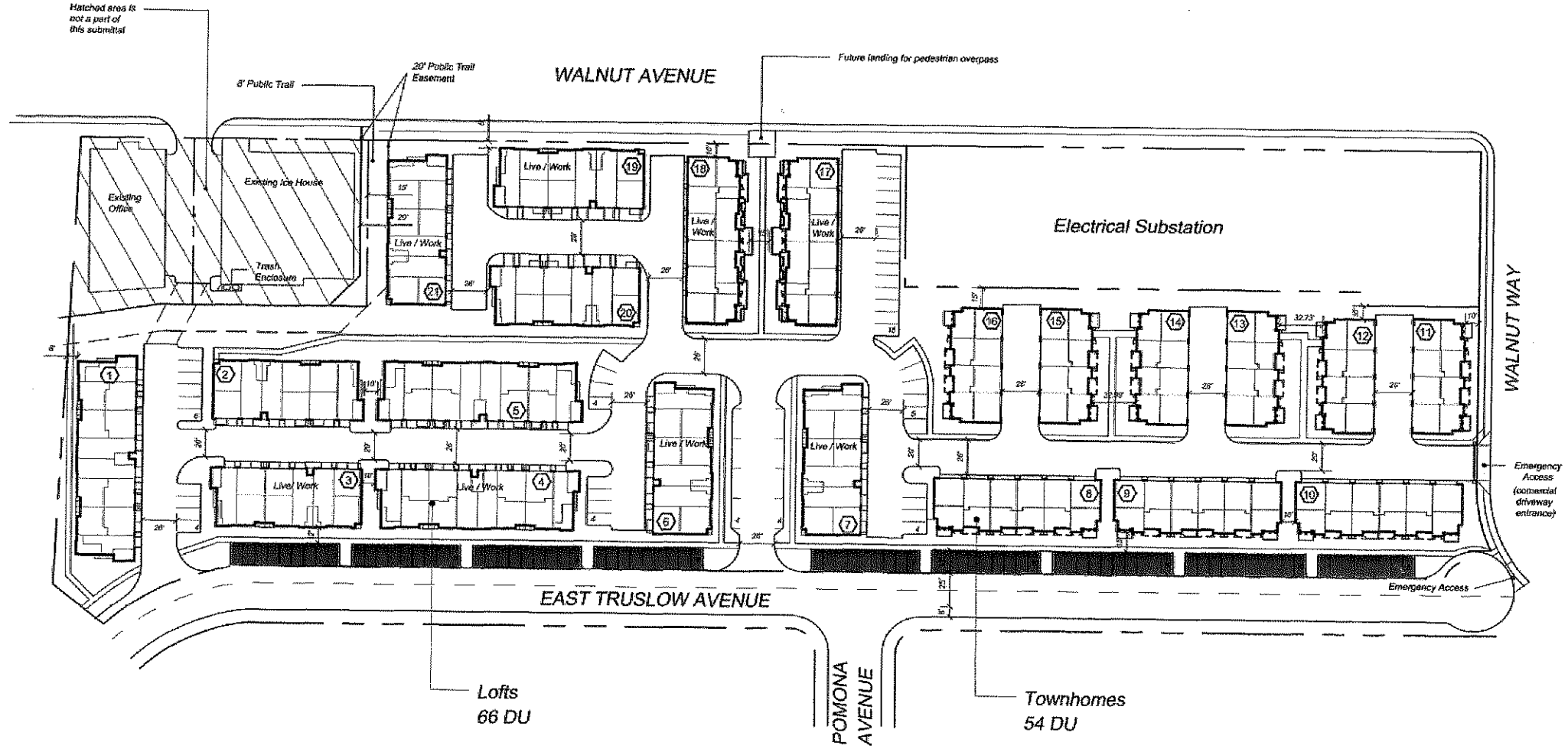
3.4.3 Drainage

Existing drainage facilities are located on the westerly side of the project. A 42 inch diameter storm drain is located in the westerly alley paralleling Harbor Blvd. This pipe picks up drainage from some existing grate inlets in the alley as well as two catch basins in Truslow Avenue. South of the project site,

the pipe is 45 inch in diameter and then ties into a box structure in Harbor Blvd. just north of Valencia Drive. Proposed storm drains will be constructed in locations depicted on Exhibit 10, Conceptual Drainage Plan, to service the Fullerton Transit Village community. The existing 42 inch line will be replaced with a reinforced concrete pipe about 45 inch – 48 inch in diameter. An 18 inch RCP will be constructed in Walnut Avenue from existing Pomona Avenue to the new 45 inch RCP. Additional off-site improvements south of the project site may be needed to convey the drainage down to the box. Additional studies will be needed to determine the final design of the systems.

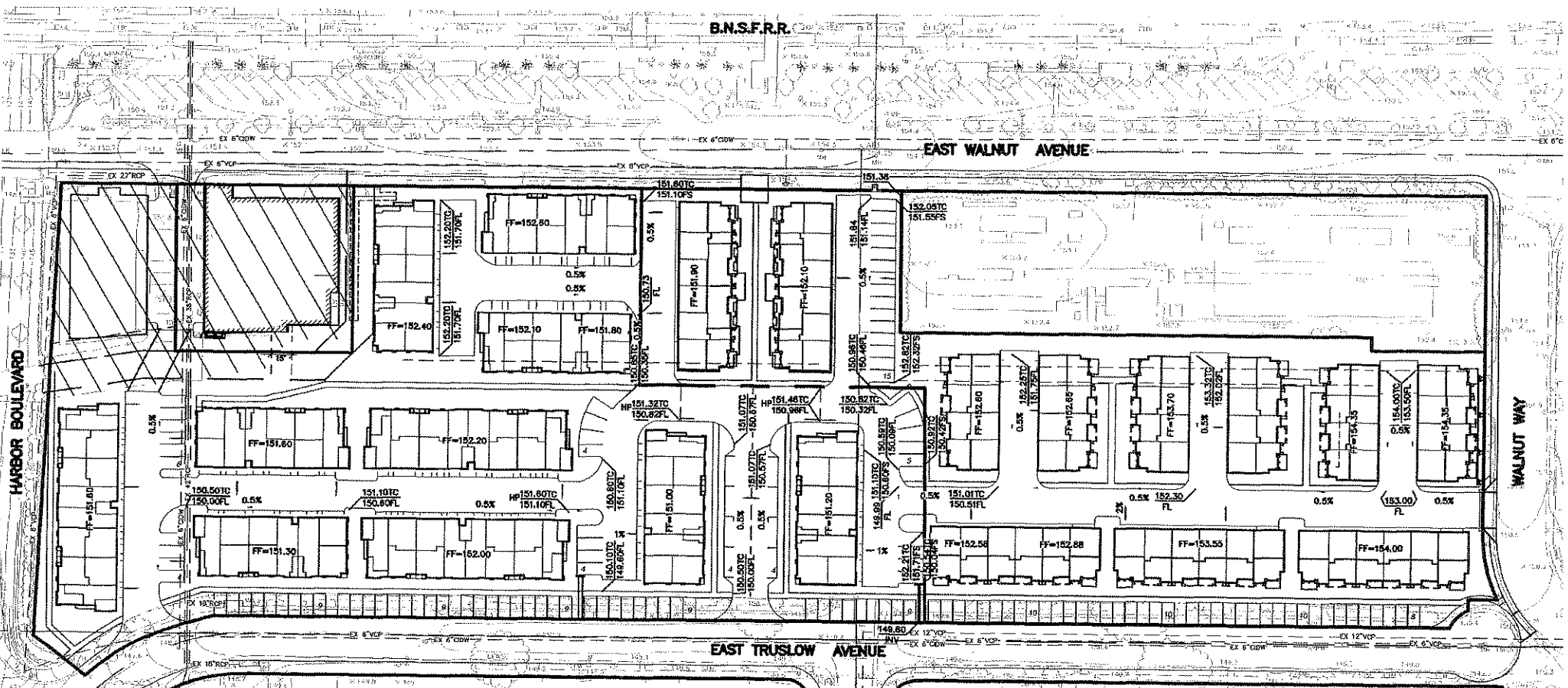
3.4.4 Utilities

Electricity is provided by Southern California Edison. Service to the proposed project will be provided through an existing system at the intersection of Pomona Avenue and East Truslow Avenue. Improvements to electrical lines to serve the project will be undergrounded, including the 66 KV line.



83 PARKING SPACES
AVAILABLE TO THE PUBLIC
6:00AM - 7:00PM

EXHIBIT 6a
Parking Plan



BNSF.R.R.

EAST WALNUT AVENUE

EAST TRUSLOW AVENUE

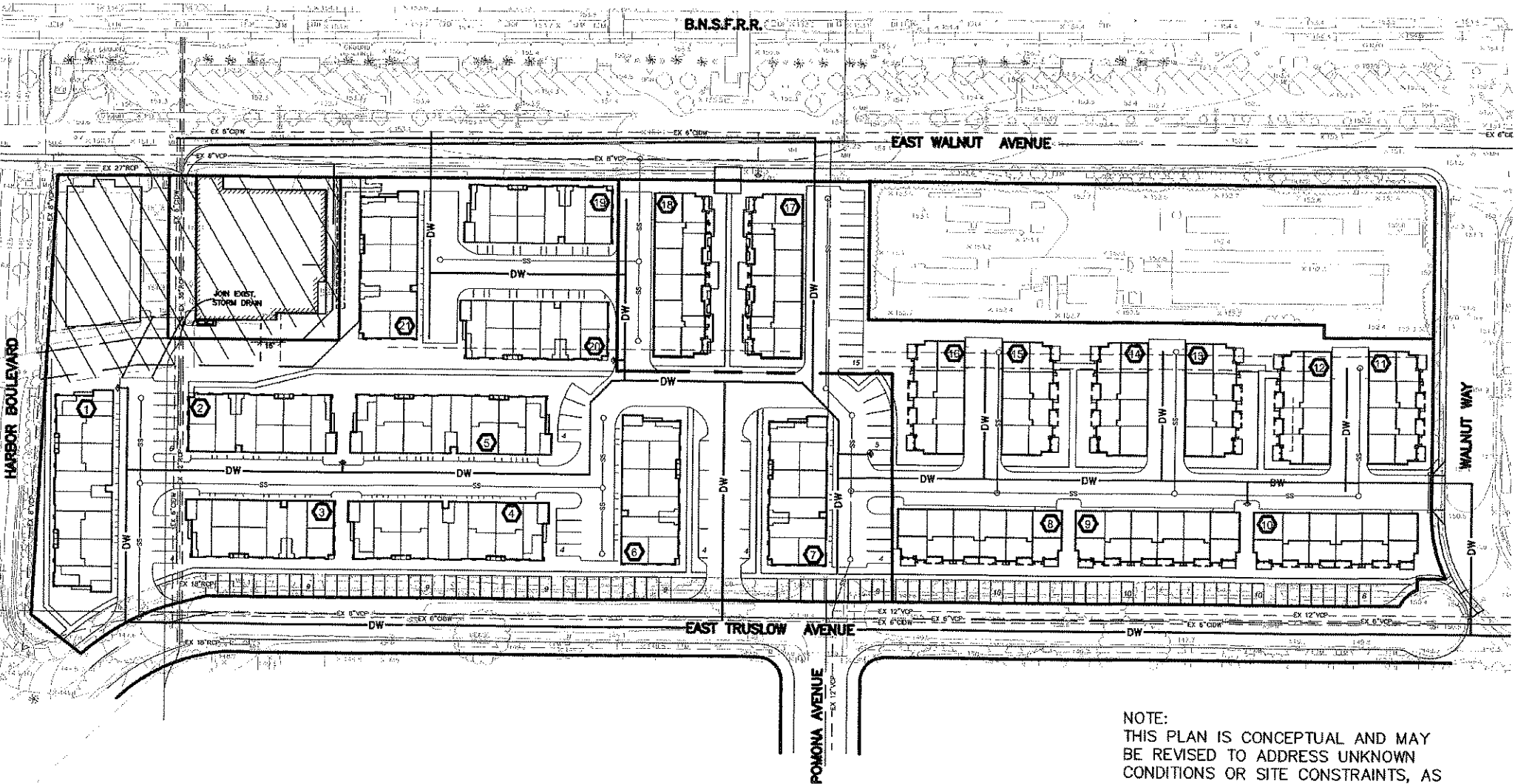
POMONA AVENUE

WALNUT WAY

NOTE:
 THIS PLAN IS CONCEPTUAL AND MAY
 BE REVISED TO ADDRESS UNKNOWN
 CONDITIONS OR SITE CONSTRAINTS, AS
 DETERMINED BY THE DIRECTOR OF ENGINEERING



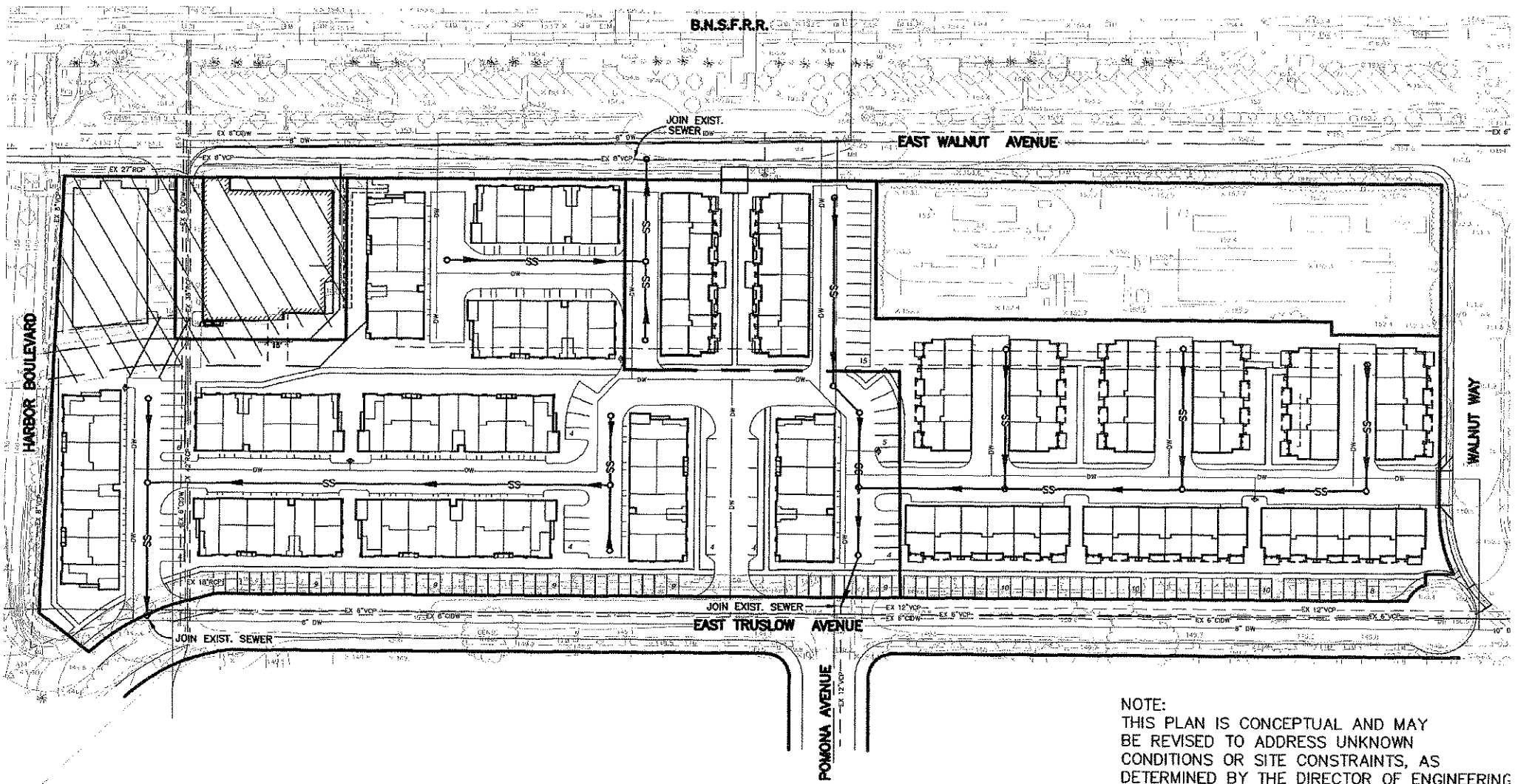
EXHIBIT 7
 Conceptual Grading Plan



NOTE:
 THIS PLAN IS CONCEPTUAL AND MAY
 BE REVISED TO ADDRESS UNKNOWN
 CONDITIONS OR SITE CONSTRAINTS, AS
 DETERMINED BY THE DIRECTOR OF ENGINEERING



EXHIBIT 8
 Conceptual Water System Plan



B.N.S.F.R.R.

EAST WALNUT AVENUE

EAST TRUSLOW AVENUE

HARBOR BOULEVARD

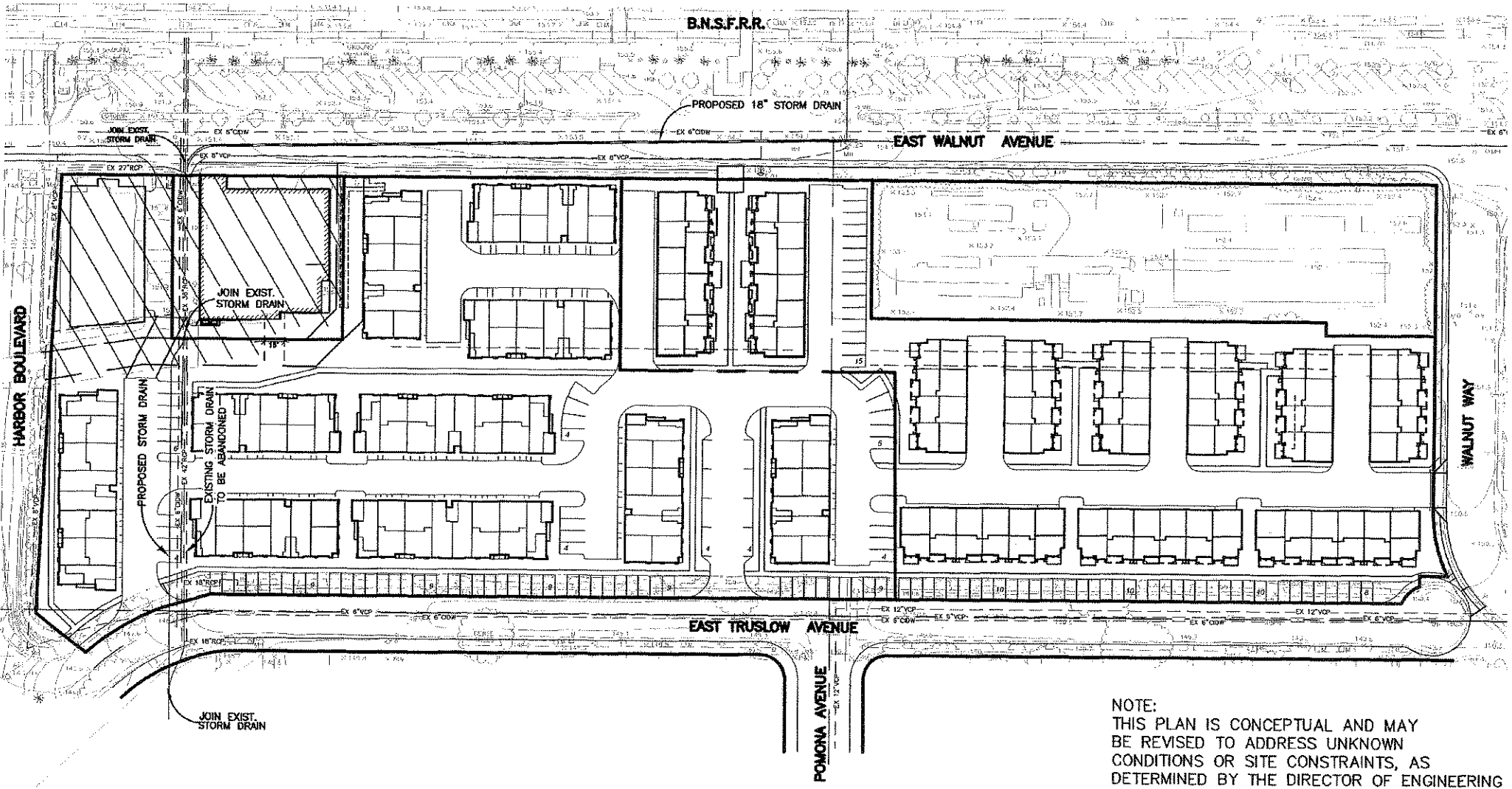
WALNUT WAY

POMONA AVENUE

NOTE:
 THIS PLAN IS CONCEPTUAL AND MAY
 BE REVISED TO ADDRESS UNKNOWN
 CONDITIONS OR SITE CONSTRAINTS, AS
 DETERMINED BY THE DIRECTOR OF ENGINEERING



EXHIBIT 9
 Conceptual Sewer System Plan



NOTE:
 THIS PLAN IS CONCEPTUAL AND MAY
 BE REVISED TO ADDRESS UNKNOWN
 CONDITIONS OR SITE CONSTRAINTS, AS
 DETERMINED BY THE DIRECTOR OF ENGINEERING



EXHIBIT 10
 Conceptual Drainage System Plan

Natural gas service is provided by Southern California Gas Company. Service to the proposed project will be provided through an extension of an existing system at the intersection of Pomona Avenue and East Truslow Avenue.

Telephone service is provided by SBC / Pacific Bell. Service to the proposed project will be provided through an extension of an existing system at the intersection of Pomona Avenue and East Truslow Avenue. An existing 10 inch gas line from Truslow Avenue to Walnut Avenue will be protected in place, or relocated.

Solid waste disposal service is provided to the project site by the MG Disposal (Taormina Industries). It is anticipated that homes will rely on individual container pick-up, provided all dwelling units are accessible to collection vehicles. Should site design preclude collection vehicle circulation, shared trash enclosures shall be incorporated into the project to the satisfaction of the City and MG Disposal.



SECTION **F O U R**

Development Standards

- 4.1 General Regulations and Conditions
- 4.2 Permitted Uses
 - 4.2.1 Permitted Uses
 - 4.2.2 Uses Permitted Subject to a Conditional Use Permit
 - 4.2.3 Accessory Permitted Uses
 - 4.2.4 Uses Permitted Subject to a Conditional Use Permit
- 4.3 Residential Developmental Standards
- 4.4 Supplemental Standards
- 4.5 Parking Standards
- 4.6 Accessory Structures and Uses
- 4.7 Signage

4.0 DEVELOPMENT STANDARDS

4.1 General Regulations and Conditions

The Fullerton Transit Village Specific Plan is intended to promote redevelopment of the project site into a new residential development. The proposed regulations guide the development so that it becomes compatible with the surrounding community, thereby creating a high quality residential environment and installing new vitality to a blighted area. The project will draw upon a design vocabulary from successful residential communities across the nation and the unique characteristics of the sites surrounding structures, and set forth permitted uses and development standards for the Specific Plan area. This Specific Plan proposes deviations from the existing City zoning standards to create a varied and imaginative overall project design. In order to implement the Fullerton Transit Village Specific Plan, a zone change to "Specific Plan" was approved by the Fullerton City Council on April 20, 2004.

The development regulations included herein shall apply to areas within the Specific Plan boundaries only. The Specific Plan development regulations shall supersede the provisions, standards and requirements of the Fullerton Zoning Ordinance (Municipal Code Title 15), except in those instances where the Specific Plan development regulations remain silent or unclear. In instances where the Specific Plan development regulations do not address specific issues or other relevant considerations, the "R-5" Zoning category shall apply.

4.2 Land Use Regulations

All uses not listed as permitted uses or as conditionally permitted uses shall be prohibited unless otherwise determined by the Director of Development Services.

4.2.1 *Permitted Uses (General)*

- Attached residential dwellings
- Small family home community care facility for the elderly
- Small family child care home

- Home business, subject to City of Fullerton Zoning regulations for “home occupations”
- Temporary model home and subdivision sales office (subject to site plan review)
- Temporary subdivision sales signage, in accordance with the Fullerton sign ordinance

4.2.2 *Uses Permitted Subject to a Conditional Use Permit (General)*

- Boarding or rooming house and similar group quarters
- Child care center (15 or more children)
- Large daycare home for the elderly (15 or more persons)
- Electric distribution substation, pumping station, water well, water reservoir
- Parking lot
- Modifications to the exterior of a building or new exterior construction, except when determined

minor by the City’s Director of Development Services

- Recreational area, lighted

4.2.3 *Home Occupations / Business (General)*

- Home businesses in residential units shall be permitted, subject to the Home Based Business regulations of the City of Fullerton Zoning Ordinance, except that the following additional uses shall be identified as clearly prohibited:
 - Uses that sell commodities on the premises.
 - Any use which is clearly not incidental and secondary to the use of the dwelling for dwelling purposes and/or changes the character thereof.
 - Escort Services
 - Massage and Acupuncture establishments

4.2.4 Permitted Uses (in Live/Work Dwelling Units)

Note: This section is supplemental to Section 4.2.1, Permitted Uses (General), and all additional commercial activities specified herein shall be restricted to the ground floor of each dwelling unit. All dwelling units shall remain residential in nature.

4.2.4.1 Definition of Live/Work

“The mixing of a residential dwelling and a commercial activity, wherein the residential and commercial spaces are clearly identified and separated, and all uses are in compliance with applicable government codes. Allowed live/work commercial activities are those which support the immediate residential neighborhood, are compatible with the adjacent residential uses, and provide a unique, vibrant community.”

4.2.4.2 Permitted Uses (Live/Work Units)

Permitted uses in live/work units shall include:

- All uses listed as permitted uses under Section 4.2.1

- Convenience shops as identified in the Fullerton Municipal Code Section 15.17.030.P.
- Public Access Businesses. Public Access Businesses typically have frequent one-on-one interaction with individual client groups who meet in home offices. Example of these types of permitted uses are:
 - Architect / Landscape Architect / Engineer / Land Planner
 - Interior Decorator or Designer
 - Video Producer
 - Fine Arts Studio and Sales/Ceramics and Pottery Studio and Sales/Clothing Design Studio and Sales
 - Graphic Design Studio and Sales
 - Photography Studio/Portraiture and Sales
 - Planning Consultant
 - Attorney/Consultant
 - Income Tax Service/Escrow Service/Insurance Agency
 - Internet Service Provider/Webmaster
 - Consulting and Business Service

- Music or Dance Instruction (tutor)
- Real Estate Developer/Specialty Contractor
- Licensed Small-family Child-Care Home
- Barber and Beauty Salon and Sales
- Personal Fitness Training
- Tutor

- Mobile Businesses. Mobile businesses typically involve a significant time away from the office where work is either acquired or performed at the client's residence or place of business. Examples of these types of permitted uses are:

- Pick-up and Delivery Service
- Cleaning Service
- Pool Maintenance
- Building Contracting
- Gardening and Landscape Service
- Electronic and Computer Equipment Repair and Fix-it Service
- Catering Service
- Flower Arranging and Plant Service
- Specialty Food Products and Delivery

- Other Permitted Businesses. Notwithstanding the specific permitted uses outlined above, the Board and City of Fullerton Director of Development Services may authorize other uses using reasonable discretion, as long as such other uses are not otherwise precluded by law. The Board and City of Fullerton Director of Development Services will consider the effect on the project, and will not approve a use that has a materially adverse impact on other units in the condominium project, or surrounding neighborhood. The Director's decision may be appealed to the Planning Commission or the Director may refer the request to the Planning Commission as a Conditional Use Permit.

4.2.4.3 Uses Conditionally Permitted (Live-Work Units)

Note: This section is supplemental to Section 4.2.2. All dwelling units shall remain residential in nature.

The following uses shall be permitted, subject to review and approval by the Director of Development Services. The Director of Development Services may establish conditions and restrictions as necessary to ensure the general welfare of the condominium neighborhood and surrounding community.

- Food preparation facilities;
- Dine-in and Take-out Restaurants;
- Coffee shops;
- Live, amplified, or outdoor entertainment;
- Other uses not listed in the permitted use category which the Director of Development Services determines may be appropriate under specific circumstances.

Requests for approval of conditionally permitted uses shall be submitted in writing to the City of Fullerton Director of Development Services. Requests shall include hours of operation, a description of the uses and products, approval from the HOA, and a map/floor plan.

4.2.5 Prohibited Uses

Prohibited uses are those uses that are not compatible with the permitted uses for the project, as well as all uses which are contrary to any city ordinance or other governmental condition of approval for the project. The following uses are expressly prohibited:

- Medical/Dental Office
- Chiropractic/Acupuncture Service
- Massage/Acupressure Service
- Veterinarian/Kennel/Animal Care Facility
- Tattoo or Body Piercing Service
- Fortuneteller
- Banquet Facility
- Adult Business
- Recycling Center
- Sales, repair or maintenance of vehicles, including automobiles, boats, motorcycles, aircraft, trucks, or recreational vehicles, provided that light maintenance of resident owned vehicles shall be allowed so long as

such maintenance is conducted entirely within the interior of a garage

- Trade or Private School
- Religious Institution
- Any use that regularly or periodically generates vibrations, excessive noise, heat or smell, which affects any other condominium units within the project, as determined by the Board; or surrounding properties, as determined by the City of Fullerton Director of Development Services
- Other uses that the Board reasonably determines would detract from the overall image of the project or which might adversely affect the value of the individual condominiums within the project

4.3 Residential Development Standards

Table 2, Development Standards, follows and sets forth the building setbacks, height, lot coverage and other requirements for development of the Fullerton Transit Village Specific Plan.

Additional development standards within the Specific Plan area follow.

4.4 Supplemental Standards

4.4.1 *General*

- City entitlements or licenses, including but not limited to building permits, conditional use permits, and business licenses shall not be reviewed or granted without first obtaining verification of approval by the Fullerton Transit Village Homeowners Association (H.O.A.)
- Temporary modular buildings such as portable buildings or mobile homes are prohibited.
- Trash receptacles shall be enclosed and, when visible from second story residences, be provided with overhead screening. All receptacles shall be screened on at least 2 sides by landscaping, and shall be constructed to City of Fullerton standards.

4.4.2 *Landscaping*

- All required landscape areas shall be provided with permanent and adequate (commercial grade irrigation where appropriate) means or irrigation. All public or common landscaping shall be installed and maintained in such a manner as to not interfere with maintenance or repair or any utility, not restrict pedestrian or vehicular access, and not constitute a traffic hazard due to reduced visibility.
- All landscaping and irrigation improvements shall be installed by the developer, and maintained by the project's Homeowners Association, and shall not be altered by any homeowner without prior consent or approval by such H.O.A. Site landscaping and irrigation improvements shall include the City of Fullerton Trail Easement and public street sidewalks and parkways abutting the project site. The City shall maintain the trail surfaces within the trail easement.

4.4.3 *Walls and Fences*

- A masonry wall, up to 8 feet in height, shall be provided around the perimeter of the Specific Plan area only at the electrical sub-station. The wall or equivalent screening is subject to approval of the Director of Development Services.
- Fencing shall require review and approval by the City's Director of Development Services. Fencing shall only be provided for security purposes, and only after consultation with the City's Chief of Police.

4.5 *Parking Standards*

Parking Standards of the City of Fullerton Zoning Ordinance shall apply to development within the Fullerton Transit Village Specific Plan area, except as defined herein.

- Aside from guest parking, it is the intent that resident vehicles shall not be left on the streets during evening hours.

- There shall be no parking in building setbacks to streets, except along East Truslow Avenue, as defined on the Parking Plan, Figure 6a.
- Accessory vehicles, such as motorcycles, motor homes, boats, personal water draft, all terrain vehicles, etc. shall be stored within an enclosed garage, or parked on site for a temporary period of 72 hours or less.
- Parking shall be prohibited in streets or alleys except where clearly marked.
- Emergency vehicle access drives and fire lanes shall be marked “no parking”.

4.6 Accessory Structures and Uses

- No accessory structure may encroach into the setbacks specified herein, unless so permitted as stated below.
- Satellite dish antennae 12 inch or less in diameter are permitted, provided they blend with the architecture of the dwelling.

4.7 Signage

The Fullerton Transit Village will require signage to promote and enhance on-site businesses, maintain a quality neighborhood, and provide direction for pedestrian and vehicular circulation.

A sign program shall be prepared and submitted for review and approval by the City of Fullerton.

Advertising of goods and services within the Specific Plan area shall comply with this sign program.

Interior or exterior signage or display of products of any type, size or style shall be permitted in association with any live/work units commercial use established within the Fullerton Transit Village Specific Plan area, subject to approval by the City of Fullerton Director of Development Services, and conformance with a City approved sign program.

TABLE 2 Development Standards		
Standard	MF	MF
	Lofts	Townhomes
Minimum Lot Area (sq. ft.)	5.40 ac total	
Maximum Project Density (units/acre)	120 d.u./22.2 d.u. per acre	
Maximum Building Height (feet/stories)	40'3 stories	
Building Setbacks		
From public street (ft.) ¹	5'	10'
Side of bldg. to property line (ft.) ²	8'	8'
Rear of bldg. to private street (ft.) ²	2'.6"	2'.6"
Rear of bldg. to alley	2'.6"	1'.0"
Front of bldg. to Front of bldg.	27.5'	32.7'
Garage to Garage	28'	28'
Minimum Distance between buildings (ft.)	10'	10'
Minimum Back-up Space for Parking	25'	25'
Minimum Unit Size (sq. ft.)	1546	1346
Usable Open Space/Unit	67 s.f.	67 s.f.
Maximum Perimeter Fence/Walls Height (ft.) (only @ sub-station)	8'	8'
Minimum parking spaces in a garage per unit	2/unit	2/unit
Minimum guest parking spaces per unit	1/unit	1/unit

Footnotes:

1. Private patios may encroach into the 10' front yard setback (townhomes), to a maximum of 6 feet, provided they are uncovered; entries may encroach into setbacks a maximum of 2 feet.
2. Minimum setback to the electrical substation shall be 10 feet.

Notes:

- Maximum building height includes chimneys.
- Side yard setback excludes exterior projections such as bay windows and chimneys which may encroach a maximum of two (2) feet.
- Patio covers and parking shall not encroach into the minimum required building setbacks. Cantilevered balconies may encroach into street setbacks. Cantilevered balconies shall not encroach into streets or alleys.



SECTION FIVE

Design Guidelines

- 5.1 Site Planning
- 5.2 Architectural Character
 - 5.2.1 Building Mass
 - 5.2.2 Roof Forms and Window Treatment
 - 5.2.3 Materials and Colors
 - 5.2.4 Additional Building Components
 - 5.2.5 Walls and Fences
- 5.3 Landscape Architecture
 - 5.3.1 Landscape Concept
 - 5.3.2 Streetscapes
 - 5.3.3 Project Entries
 - 5.3.4 Plant Palette
- 5.4 Auxillary Structures / Equipment and Utilities

5.0 DESIGN GUIDELINES

The Fullerton Transit Village Specific Plan area is envisioned as an urban infill project providing a blend of multi-family residential products, and offering live/work opportunities. Design guidelines provide an overall vision for development of the Specific Plan area, and will guide implementation of the plan as building occurs. The intent of these guidelines is to establish a design framework and criteria for designers and developers to use as a guide, and which the City of Fullerton will use to evaluate development within the Specific Plan area. These guidelines will assure the City of Fullerton that individual improvements will conform to a high standard of design, ensure compatibility with the surrounding community, and enhance the overall image of the City. Guidelines for site planning, architectural character and landscape architecture are provided to create a cohesive neighborhood while providing diversity throughout the community.

5.1 Site Planning

- The general plotting concept, as shown on Exhibit 5, Land Use Concept provides layout, location, and specific product orientation. Modification may be approved as long as the overall project density and residential mix are maintained within the Fullerton Transit Village Specific Plan.
- The overall community design concept for the Fullerton Transit Village at Fullerton is that of a pedestrian-friendly transit oriented lifestyle that reflects the eclectic, urban nature of the elements surrounding the site. Pedestrian linkages play a dominant role in the site design, and a central paseo provides direct access to the Fullerton Train Station through an existing pedestrian overpass. The focus of the site elements will be oriented outward, with residences facing toward the surrounding perimeter streets.

5.2 Architectural Character

Two historic structures provide the inspiration for the architectural character of the project. The Fullerton Train Depot, located to the north of the site, across the railroad tracks, is a Spanish Colonial style structure built in 1930. The Crystal Ice House, a brick commercial structure constructed in 1910, is directly adjacent to the northwest portion of the site. The proposed residences will represent interpretations of both the Industrial and Spanish Colonial styles, thereby reflecting the historic architectural diversity of the surrounding neighborhood.

Spanish Colonial

The Spanish Colonial Revival style, also known as Spanish Eclectic, is an adaptation of the Mission revival enriched with additional Latin American details and elements. The style attained widespread popularity after its use in the Panama-California Exposition of 1915. Architectural distinction is established in the Spanish Colonial Style through the use of tile roofs, stucco walls, exterior arches, and ornamental ironwork.

The charm of this style lies in its directness, adaptability, and contrast of materials and textures.

The Fullerton Depot building exemplifies this style through its use of rose-colored stucco, barrel tiled roofs, recessed, arched windows, and fanciful ironwork. Other outstanding decorative features include keyhole shaped Moorish windows, Mediterranean tile, and scalloped projections with ornate corbels. The style of this historic building brings a unique flavor to the community that will be reflected in the proposed architecture for the Fullerton Transit Village site.

Industrial Style

The origination of today's Industrial style grew out of the Industrial Revolution of the nineteenth century, when many large factory and warehouse structures were constructed. These buildings needed to be large enough to accommodate manufacturing processes, and usually were located near train stations or waterways to ease transportation of raw materials and products. The decline of heavy industry has made the

original function of the structures obsolete. Some of these buildings have been reclaimed as residential “lofts” to provide housing for a growing inner city population. These converted lofts have been so successful that their linear, urban style has become popular for new home construction.

Industrial style is utilitarian by nature, with simplified ornamentation. Brick is the predominant material used, and roof forms are usually flat. The Ice House structure is true to the Industrial style, constructed completely of brick, with recessed wood windows, a brick cornice with a basic dental design, and a “frame” pattern insert. This structure lends the Fullerton Transit Village site a unique sense of place and historic elements, that can be reflected in the architecture of the proposed residences.

5.2.1 Building Mass

The overall urban character of the site calls for the building massing to have a significant presence along perimeter streets, and for front doors to be oriented outward. Where appropriate and authentic to style, portions of the building should provide

vertical and horizontal massing breaks to encourage proportional diversity. The concept of incorporating massing variations into the building’s façade provides architectural interest, shade and shadow opportunities and building relief.

5.2.2 Roof Forms and Window Treatment

The dominant impact to the skyline or project background is the shape of the building roofline. Articulation of the building mass rooflines will express a variety of conditions and minimize the visual impact of repetitious flat planes and roof shapes. This can be achieved by using a variety of roof forms including shed, hipped and parapet/flat roof elements. Recessed fenestration and simple window detailing should be incorporated in the building façade to reflect and compliment the architectural style.

5.2.3 Materials and Colors

The colors and materials used for this project will reflect a general theme of warm, sun-washed colors that are in harmony with the surrounding environment and structures. A variety of



natural looking materials and colors will provide the diversity required for visual interest while supporting the community character and creating a timeless appeal. The primary purpose of the architectural color palette selection is to avoid monotony, provide a variety of color schemes and promote visual diversity.



Townhomes 5-plex
Exterior Front Elevation



Townhomes 5-plex
Right-side Elevation



Townhomes 5-plex
Left-side Elevation



Townhomes 5-plex
Exterior Rear Elevation

EXHIBIT 11b
Conceptual Architectural Elevations



Loft 8-plex
Exterior Front Elevation



Townhomes 6-plex
Exterior Rear Elevation



Townhomes 6-plex
Exterior Front Elevation

EXHIBIT 11c
Conceptual Architectural Elevations



Loft 8-plex
Side Elevation: Alt1



Loft 8-plex
Side Elevation: Alt2



Loft 8-plex
Exterior Rear Elevation

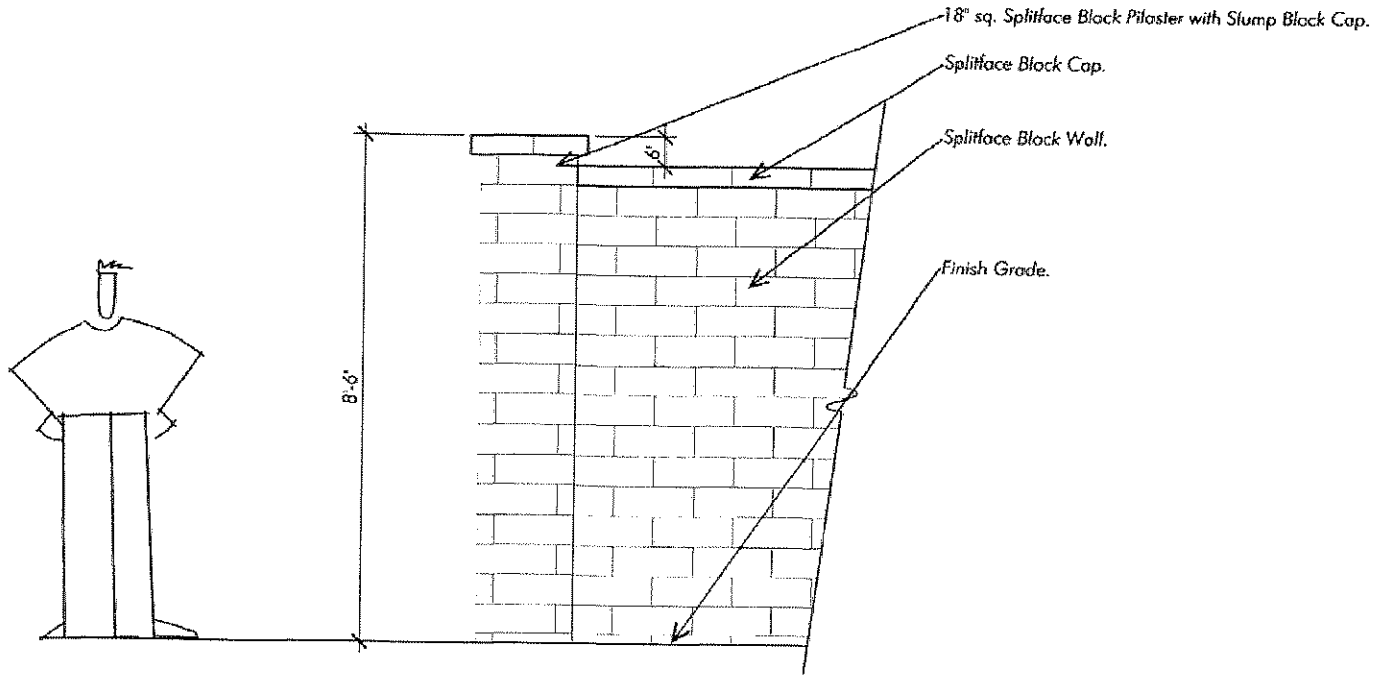
5.2.4 Additional Building Components

Accessory structures such as trash enclosures and mailboxes should be designed so that they are consistent with the architectural style of the adjacent primary structures and compatible with the residential character of the neighborhood. They should incorporate similar materials and draw from the same color palettes.

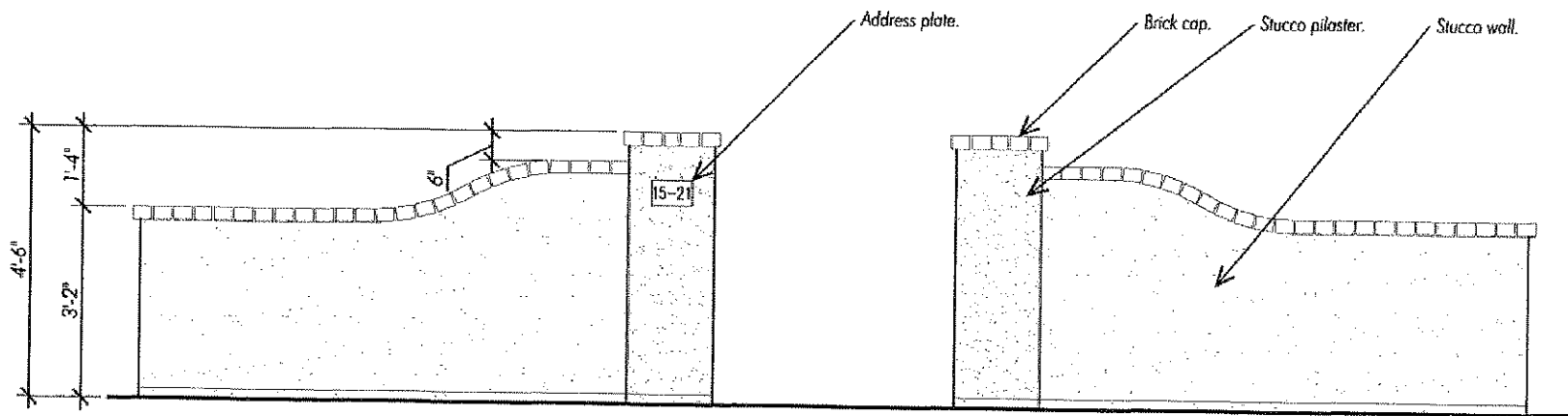
5.2.5 Walls and Fences

To create a community identity and cohesive streetscene, community walls or fences of consistent material and design will be used along portions of the perimeter of the Specific Plan area. Depending on their location within the community, walls and fences provide privacy, enclose public and private spaces, establish character, enhance security and safety, and minimize impacts to surrounding areas. Exhibit 12, Conceptual Walls and Fences, shows the proposed wall and fence elevations. General guidelines are provided below:

- Decorative walls and fences shall be limited to a maximum height of 96 inches (8 feet) and be constructed in concert with plant material described in the landscape plan. Specific Plan development standards establish an 8 foot maximum height for decorative screen walls at the Edison sub-station. Six foot maximum height walls may be appropriate elsewhere, as determined by the Director of Development Services. Noise attenuation walls shall be reviewed on an individual basis pursuant to a noise analysis.
- Fences or walls within a street setback or in front of dwellings should not exceed 36 inches in height.
- Walls and fences visible from public streets should be constructed of durable, high-quality materials, should display a high level of quality in finish and detail, and shall match the architecture of the adjacent buildings.
- Permitted materials include stone, stone veneer, stucco, masonry, brick, and wrought iron. Materials shall be consistent with materials reviewed and approved by the City's Redevelopment Design Review Committee (R.D.R.C.)



SPLITFACE WALL AND PILASTER



COURTYARD ENTRY

EXHIBIT 12
Typical Walls and Fences

- Prohibited materials include barbed wire, chain link, chicken wire, metal (aside from wrought iron used in view fencing), galvanized steel, corrugated metal, or aluminum fencing, plywood or other flat, non-relief surfaces.

Community walls and pilasters will be of durable material (e.g., stone, masonry or stucco-covered block) and of a design and color consistent with the residential character. These walls will be a maximum of 8 feet in height and be generously landscaped to prevent them from becoming a dominant visual element of the streetscape. Landscaping shall also include plant material to discourage graffiti and unauthorized access.

5.3 Landscape Architecture

Final landscape plans shall be reviewed and approved by the City's Redevelopment Design Review Committee (R.D.R.C.) pursuant to City Council Resolution No. 9571.

5.3.1 Landscape Concept

The community landscape concept (see Exhibit 13, Conceptual Landscape Plan) is intended to create a distinctive, diverse

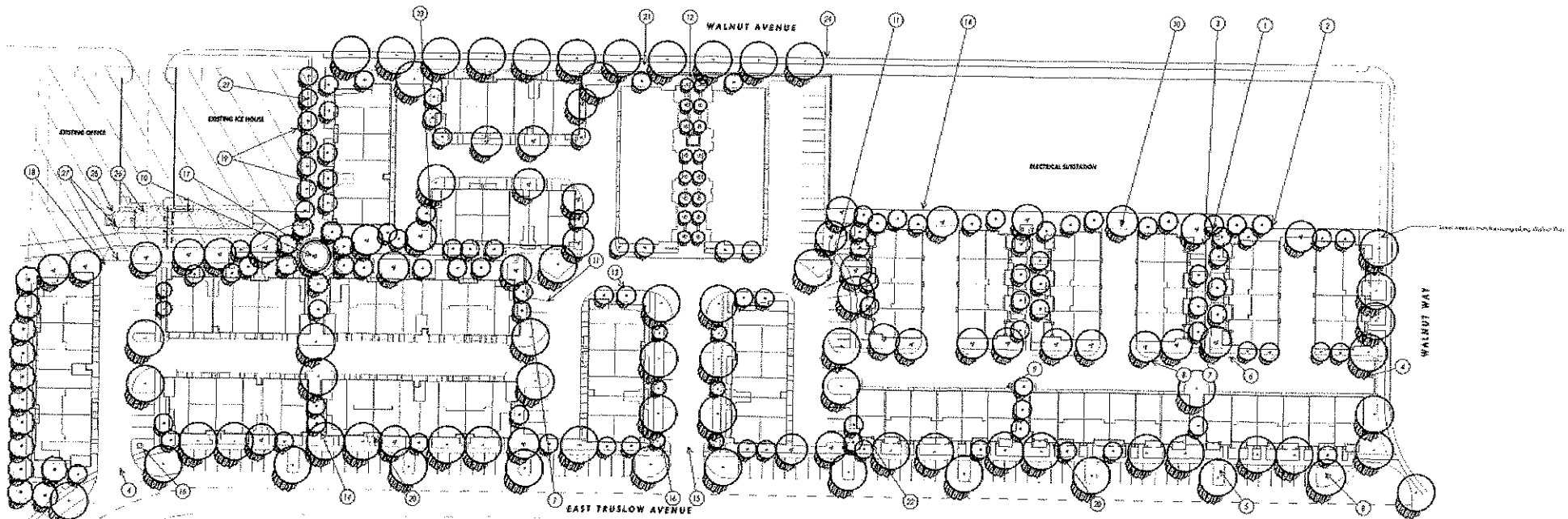
environment that will lend identity and character to the Specific Plan area. Consistent treatment of all landscaped areas is critical to the creation of a high quality outdoor environment. The proposed landscape features are designed to enhance the project's interface with the surrounding environment and the adjacent community. The landscape concept suggests methods of treatments that might be considered.

- Landscape treatments should utilize and adapt to landscape elements from the urban patterns found in the traditional neighborhoods of Southern California, including regularly planted shade trees and clustered plantings.
- Landscape treatments should incorporate colorful plant material and evergreen trees in areas where such accents are appropriate.
- Landscape improvements should require minimal maintenance and irrigation. The use of native and/or drought-tolerant plant material should be emphasized.

- All public and common areas and rights-of-way should have automatic irrigation systems (commercial grade, where applicable). Low volume spray heads and drip irrigation systems should be used and be compatible with any reclaimed water systems utilized.
- Landscape plans shall consider service lines, traffic safety sight line requirements, and structures on adjacent properties to avoid conflicts as the trees and shrubs mature. Street trees and trees planted in privately owned landscaped areas near public walkways or street curbs should be selected and installed to prevent unreasonable damage to sidewalks, curbs, gutters and other public improvements.
- Tree species with invasive root systems should be oriented away from water lines, sewer lines or public rights-of-way (see Section 5.3.4, Plant Palette).
- Landscape maintenance practices for common areas should be carried out at regular intervals sufficient to promote plant health and appearance. Maintenance may include discing, mowing, pruning, clearing of debris, weed removal, replacement of dead plant material, and

repair and replacement of non-functioning irrigation equipment.

- Street trees should not be located where they prevent effective solar access, unless they are deciduous.
- All landscaping shall be maintained to prevent interference with emergency vehicle access.
- Landscaping in the City trail shall be designed in a fashion acceptable to the City's Community Services Department, and compatible with the surrounding portions of the trail already developed.



LEGEND

1. Passive seating area.
2. Slump block pilaster.
3. Walkway.
4. Secondary entry.
5. Planting area.
6. Sidewalk.
7. Courtyard entry and address monument.
8. Street tree.
9. Drive apron.
10. Focal Tree.
11. Parking.
12. Fountain.
13. Vertical tree.
14. 8' High splitface wall.
15. Primary entry.
16. Entry monument.
17. Seat wall.
18. 20' Emergency access.
19. 20' Public trail easement.
20. Unit entry.
21. Public 10' walkway.
22. Unit Patio.
23. Turf.
24. Noise attenuation wall.
25. Trash Enclosure.
26. Turf Block.
27. Emergency Access Gates.

Tree Legend

Symbol	Botanical Name	Common Name
STREET TREE (Min. 24" box)		
	<i>Cinnamomum camphora</i>	Camphor Tree
	<i>Platanus x acerifolia</i>	London Plane Tree
	<i>Quercus ilex</i>	Holly Oak
	<i>Schinus molle</i>	California Pepper
FOCAL TREE (Min. 24" box)		
	<i>Platanus racemosa</i>	California Sycamore
	<i>Quercus ilex</i>	Holly Oak
	<i>Agonis flexuosa</i>	Peppermint Tree
	<i>Olea europaea</i>	Olive Tree
	<i>Schinus molle</i>	California Pepper

Tree Legend

Symbol	Botanical Name	Common Name
VERTICAL TREE (Min. 15 gal.)		
	<i>Cupressus sempervirens</i>	Italian Cypress
	<i>Eucalyptus spp.</i>	Eucalyptus
	<i>Melaleuca quinquenervia</i>	Cajeput Tree
	<i>Tristania conferta</i>	Bristena Box
ACCENT TREE (Min. 15 gal.)		
	<i>Chamaerops humilis</i>	Mediterranean Fan Palm
	<i>Juniperus spp.</i>	Juniper
	<i>Lagerstroemia indica</i>	Crape Myrtle
	<i>Melaleuca nesophila</i>	Pink Melaleuca
	<i>Strelitzia reginae</i>	Giant Bird of Paradise
ACCENT TREE (Min. 24 gal.)		
	<i>Arbutus unedo</i>	Strawberry Tree
	<i>Cupaniopsis anacardioides</i>	Carrotwood Tree
	<i>Schinus molle</i>	California Pepper



EXHIBIT 13
Conceptual Landscape Plan

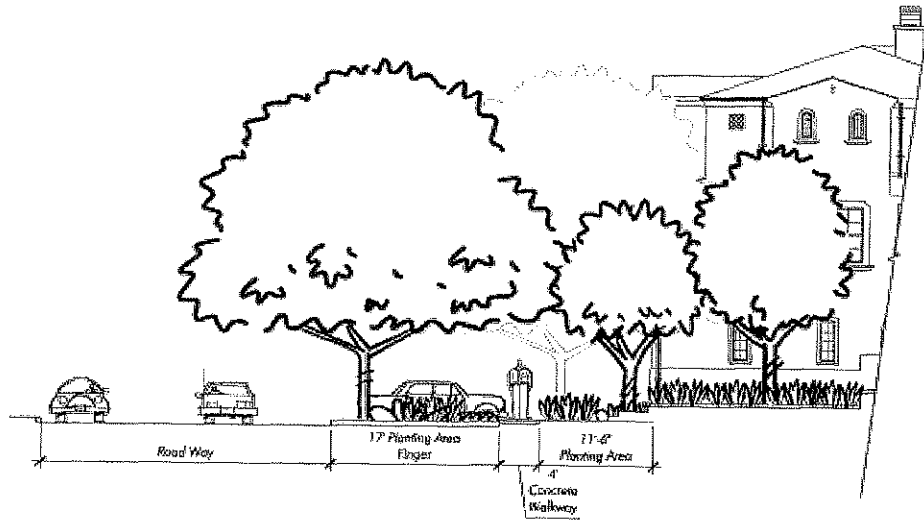
5.3.2 Streetscapes

The streetscapes within the Fullerton Transit Village project will receive a distinct landscape design. The goal is to establish the neighborhood character with a street tree palette appropriate for the scale and architecture, with particular attention to privacy and noise abatement. Courtyard planting shall generally consist of vertical screen elements and flowering accent trees.

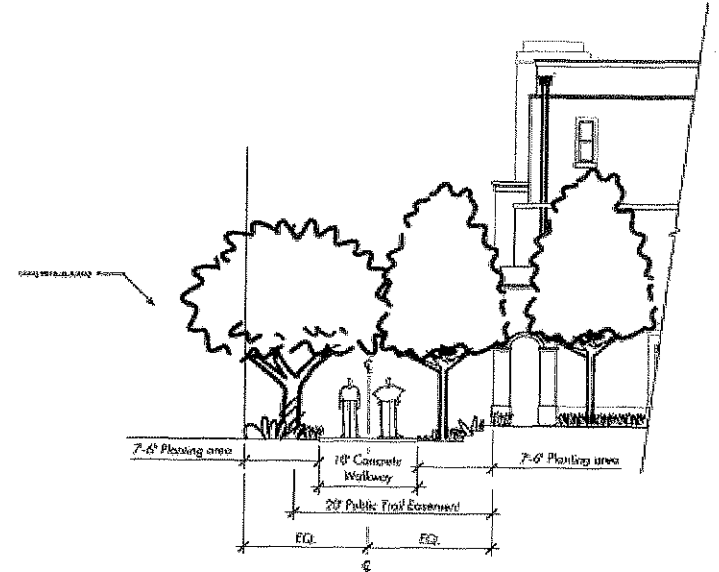
- Landscape treatments should generally utilize formally planted street trees, unless otherwise specified.
- Street tree planting should generally utilize one or two primary species for each street, with a limited number of additional accent trees which are consistent with the overall site landscape concept.
- Landscape vines and espaliers should be planted along perimeter walls to discourage graffiti.
- In order to conserve water, landscaping may emphasize the use of shrubs, groundcovers, native grasses and mulches, in addition to turf.
- Landscaping in public areas should minimize the use of turf, except in specially designed corridors, entries, and common areas used for recreation.
- Masses of groundcovers and shrubs should be limited to a few, extensively used species for each street. Variations to the basic species should selectively be used for accent and contrast.
- Street trees will be added on east Walnut Avenue to match existing trees. Palm trees matching those at the Train Depot shall be used to create a visual link and association with the Transit Center.
- Streetscape and landscaping of the trail easement on East Walnut shall be designed with an urban context, utilizing such features as street tree grates, accent and decorative paving, globe light poles, and other urban features.
- Existing trees along East Truslow will be replaced or relocated as needed. Mature canopy trees will be provided on Truslow Avenue to soften the mass of 3 story structures.



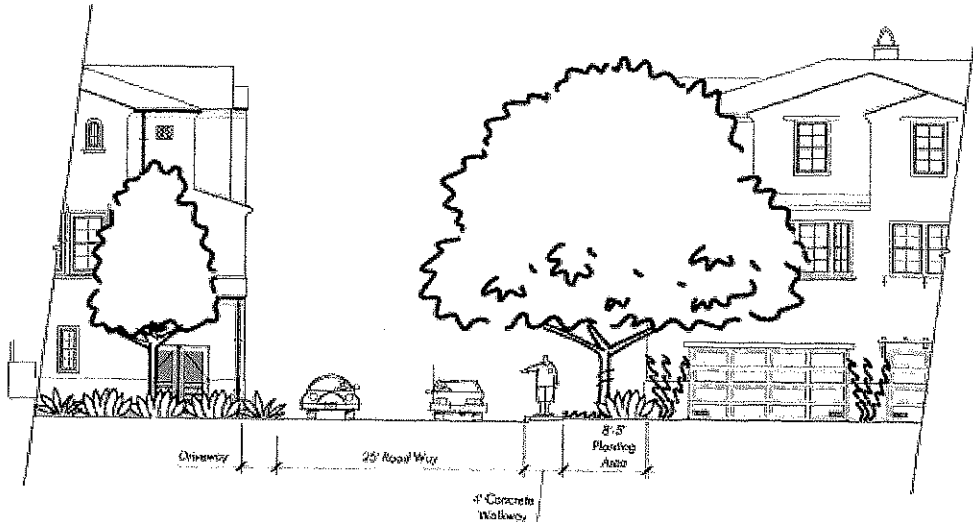
- Landscaping and landscape features shall provide whimsical, interesting, or unique accent features to create a sense of identity and place recognition.
- Refer to Exhibit 14, Typical Streetscape.



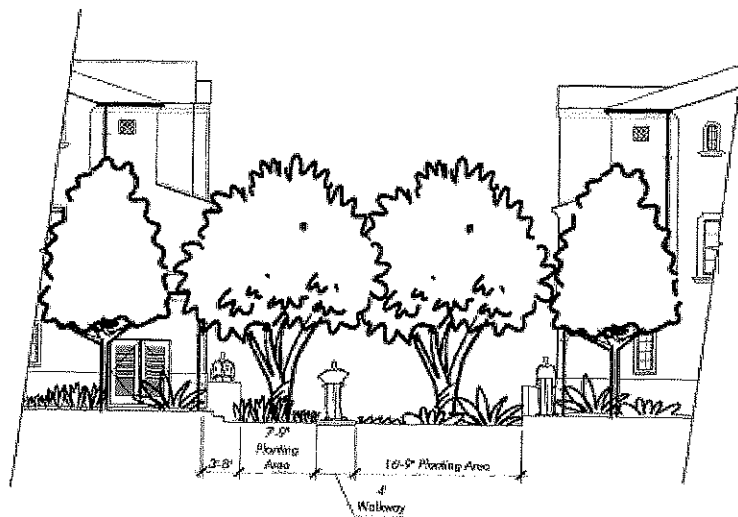
A TRUSLOW SECTION ELEVATION
Scale: 1/8" = 1'-0"



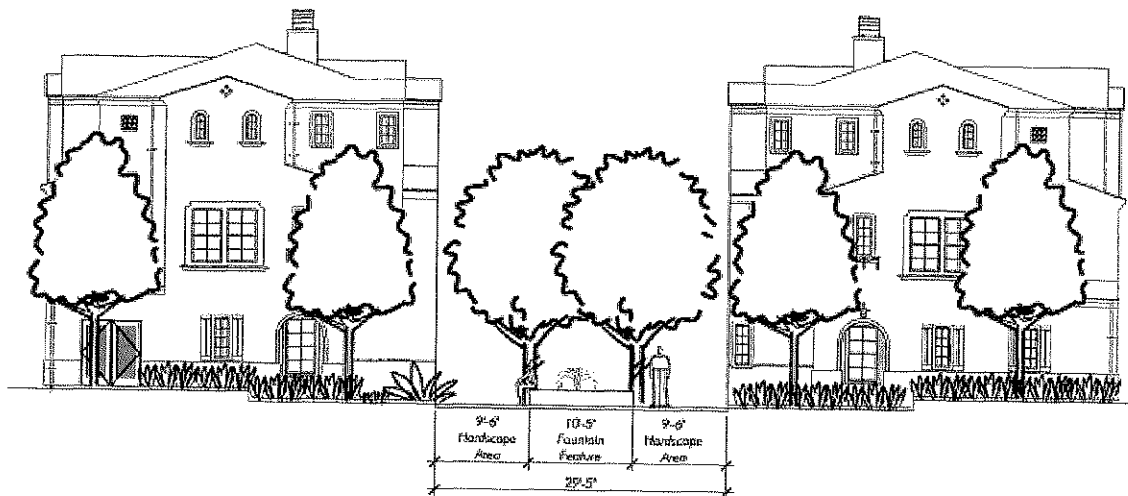
B LOFT AND PUBLIC TRAIL SECTION ELEVATION
Scale: 1/8" = 1'-0"



C INTERIOR STREET SECTION ELEVATION
Scale: 1/8" = 1'-0"



D COURTYARD SECTION ELEVATION
Scale: 1/8" = 1'-0"



E LIVE/WORK SECTION ELEVATION
Scale: 1/8" = 1'-0"

EXHIBIT 14a
Typical Streetscape

5.3.3 *Project Entries*

Special design emphasis is given to the main entry into the site. The primary project entry will be enhanced with clusters of vertical screening elements and flowering accent trees on either side. The treatment of the primary and secondary entries will reinforce the overall community character while distinguishing the Fullerton Transit Village project as a unique place.

The Primary Community Entry is located off East Truslow and is the most prominent point of arrival to the project. The landscape treatment may include:

- Interesting accent shrubs and paving
- Accent paving in pedestrian or vehicular areas
- Accent lighting on landscaped and pedestrian paving areas

The Secondary Community Entry is located on East Truslow, west of the primary entry. This entry will utilize smaller

landscape treatment elements such as pilasters, walls, paving or other details established by the Primary Community Entry.

5.3.4 Plant Palette

A plant palette has been created to reinforce the character of the Specific Plan. Species should be chosen for their ability to reinforce not only the community character, but also the areas where their presence has an influence. A final plant palette shall be incorporated with the final landscape plans reviewed and approved by the City's Redevelopment Design Review Committee (R.D.R.C.)

<u>Botanical Name</u>	<u>Common Name</u>
Agapanthus orientalis	Lily of the Nile
Agave attenuata	Agave
Aloe saponaria	Soap Aloe
Alyogyne huegelii	Blue Hibiscus
Anigozanthus favidus	Kangaroo Paw
Azalea spp.	George Tabor Azalea
Bougainvillea spp.	NCN
Buxus m. japonica	Japanese Boxwood
Calliandra haematocephala	Pink Powder Puff
Clytostoma callistegioides	Lavender Trumpet
Cuphea hyssopifolia	False Heather
Cyperus papyrus	Papyrus
Dietes bicolor	Fortnight Lily
Distictus buccinatoria	Blood-red Trumpet
Echium fastuosum	Pride of Madeira
Feijoa sellowiana	Pineapple guava

<u>Botanical Name</u>	<u>Common Name</u>
Grevillea spp.	NCN
Hedera helix	English Ivy
Hibiscus rosa-senensis	Tropical Hibiscus
Lavatera assurgentiflora	Tree Mallow
Leptospermum s. Ruby Glow	New Zealand Tea
Ligustrum j Texanum	Texas Privet
Liriope gigantean	Giant Lily Turf
Marathon II	Fescue
Myrtus c. Compacta	Dwarf Myrtus
Nephrolepis cordifolia	Sword Fern
Parthenocissus tricuspidata	Boston Ivy
Pelargonium peltatum	Ivy Geranium
Phormium spp.	New Zealand Flax
Pittosporum tobira Variegata	Variegated Mock
Pyracantha spp.	Firethorn
Rhaphiolepis spp.	India Hawthorn
Rosa bankside	Lady Banks Rose
Rosa floribunda Iceberg	Iceberg Rose
Rosmarinus o. Tuscan Blue	Tuscan Blue Rose
Salvia leucantha	Mexican Bush
Sedum spp.	NCN
Senecio mandrilscae	NCN
Strelitzia reginae	Bird of Paradise
Trachelospermum jasminoides	Star Jasmine
Viburnum spp.	Viburnum Species
Vinca major	Periwinkle
Wisteria sinensis	Chinese Wisteria

5.4 Ancillary Structures / Equipment and Utilities

Additional Building Components

- Exposed machinery, essential towers, etc. not contained within a building shall be given special design consideration and shall be designed to enhance the total complex in terms of proportion, location and aesthetic impression.
- Air conditioning units shall not be visible from public or neighboring view unless they are enclosed and treated as an integral part of the overall building design.

Mechanical Equipment

- All air conditioning/heating equipment, soft water tanks, gas and electric meters, spa and fountain equipment must be screened from public view with landscaping or fencing.
- Window or rooftop mounted air conditioning units visible from neighboring properties are prohibited.
- All exterior mechanical equipment shall be screened from all sides.

- Where rooftop mechanical equipment is visible from higher adjacent properties, it should be painted to match the roof color.
- Rooftop solar water tanks are prohibited. Solar panels must be screened from view from public and common areas, dwelling units, and adjacent properties.
- Screening materials shall blend with the building materials and design, as well as with landscaping within the vicinity.
- Screening shall be in a manner consistent with the building facade.

Utilities

- All utilities within the Fullerton Transit Village Specific Plan area shall be placed underground.
- The location of above-ground appurtenant utility boxes and similar equipment, if required, shall be consolidated whenever possible. Above ground equipment shall be screened or visually minimized with landscaping.



- To the extent possible, utility meters shall be screened from view from public rights-of-way.
- Utility connections to a building must be located to be as unobtrusive as practicable, with the preference being at the side or rear.



SECTION SIX

Implementation

- 6.1 Financing and Maintenance of Improvements
- 6.2 Methods and Procedures for Implementation
 - 6.2.1 Tentative and Final Subdivision Maps
 - 6.2.2 Design Review
- 6.3 Enforcement of the Specific Plan
- 6.4 Amendments to the Specific Plan
- 6.5 Phasing

6.0 IMPLEMENTATION

6.1 Financing and Maintenance of Improvements

The financing and maintenance plan for the Fullerton Transit Village Specific Plan will ensure the timely completion of public facilities, streets, utilities, and other necessary capital improvements as well as the proper maintenance of these facilities. Some of the potential funding mechanisms for the public improvements of the Specific Plan include:

- Impact Fees
- Homeowners Association
- Conventional financing

Table 3, Financing and Maintenance Plan, which follows, indicates the parties responsible for construction, financing and maintaining the public improvements proposed by the Specific Plan.

6.2 Methods and Procedures for Implementation

The City of Fullerton shall administer the provisions of the Fullerton Transit Village Specific Plan in accordance with the State of California Government Code, Subdivision Map Act, the Fullerton General Plan and Zoning Ordinance. The Specific Plan development procedures, regulations, standards, and guidelines shall supersede the relevant provisions of the City's Zoning Ordinance, as they currently exist or may be amended in the future. Any development regulation and building requirement not addressed in the Specific Plan shall be subject to the City's adopted regulations.

There are several requests and associated procedures for the Fullerton Transit Village Specific Plan by the City of Fullerton. The first level is the adoption of the Specific Plan document, a lot line adjustment, and change to a Specific Plan zoning category. The second implements the site component of the plan through a Tentative Tract Map and street abandonment. The third is a review of the development itself through a Development Project Application. The approval of the project

will be by the City Council, with detailed plans submitted for final administrative approval.

6.2.1 Tentative and Final Subdivision Maps

All subdivision maps of any type shall be submitted, reviewed and approved in accordance with the City of Fullerton Subdivision Ordinance and the California Subdivision Map Act. For projects requiring a tentative tract or parcel map(s), the provisions and procedures of this Specific Plan, and notwithstanding the procedures of the City's Subdivision Ordinance shall apply. A tentative tract map or parcel map may be processed concurrently with this Specific Plan.

Applications for tentative and final subdivision maps, parcel maps, and lot line adjustments are to be filed with the Engineering Department for processing as outlined in the City of Fullerton Municipal Code. The Director shall review the application for completeness, and either accept the application as final or mail a written notification of application incompleteness to the applicant within 30 days of submittal. Once an application has been deemed complete, it shall be

reviewed and processed according to the provisions of California Government Code Section 65090 et seq.

6.2.2 Design Review

Prior to building permit issuance, final site, architectural and landscape architecture plans shall be reviewed administratively by staff, the Staff Review Committee, Redevelopment Design Review Committee, the Planning Commission, or the City Council, as established by the City Council, Resolution #9571, approving this Specific Plan, or any applicable development project.

6.3 Enforcement of the Specific Plan

The enforcement of the provisions of this Specific Plan shall be the following:

- The Director of Development Services shall be responsible for interpreting and enforcing the site development standards and design guidelines set forth herein.

- The Planning Commission may review any administrative interpretation of the Fullerton Transit Village Specific Plan, subject to appeal to the City Council.

6.4 Amendments to the Specific Plan

Any proposed changes to the Fullerton Transit Village Specific Plan that would substantially alter the land use concept contained within this document shall be considered amendments to the Specific Plan and shall be processed pursuant to provisions contained in Government Code Section 65453, and in the same manner as a zoning ordinance text amendment. The Director of Development services may approve revisions to this document in order to clarify inconsistencies or ambiguities.

The Planning Commission and City Council shall find, in approving or conditionally approving an amendment, that there is not a conflict with the purpose and intent of the Fullerton Transit Village Specific Plan. Specific Plan amendments shall be consistent with the Fullerton General Plan and Zoning

Ordinance, where applicable. Once the proposed amendment has been submitted to the Department of Development Services, City staff shall make a determination on the completeness of the submittal, then an amendment application shall be processed pursuant to the City's procedure for amending the Zoning Ordinance (F.M.C. Chapter 15.72)..

6.5 Targeted Phasing

The Fullerton Transit Village project will be completed in up to 10 phases, over a 24 month period. The first phase shall be initiated by March 2005 and the last phase will be completed by September 2006. Each subsequent phase will be implemented within 4 months of the previous phase completion, and completed 8 months thereafter. However, utilities, streets, and alleys may be required outside the phase limit, subject to the review and approval of the Director of Engineering.

**TABLE 3
Financing and Maintenance Plan**

Service or Facility	Party(ies) Executing Construction	Party(ies) Financing Construction	Party(ies) Responsible for Operation and Maintenance
On-site Residential Streets (Private)	Developer	Developer	Homeowners Association
On-Site Water, Sewer and Storm Drainage Facilities	Developer	Developer	Utility Operator City (on-site drainage to be maintained by the Homeowners Association)
Off-site ROW Improvements	Developer	Developer	Homeowners Association (City shall maintain street pavement and street sidewalks)
On-Site Improvements	Developer	Developer	Homeowners Association (including landscaping and trail/sidewalk surfaces)
City Trail Easement Improvements	Developer	Developer	Landscaping by Homeowners Association. Trail and sidewalk surfaces by City



SECTION **S E V E N**

Consistency with the Fullerton General Plan

- 7.1 Land Use Element
- 7.2 Circulation
- 7.3 Housing
- 7.4 Resource Management
- 7.5 Community Health & Safety
- 7.6 Regional Coordination

7.0 CONSISTENCY WITH THE FULLERTON GENERAL PLAN

The purpose of this chapter is to ensure that the Fullerton Transit Village Specific Plan is consistent with the goals and policies of the City of Fullerton's General Plan, as required per Government Code Section 65454. A general discussion of how the Specific Plan conforms to the pertinent goals and objectives identified in the General Plan is provided herein. Only those goals and objectives that apply to or have relevance to the Fullerton Transit Village project are discussed.

7.1 Land Use

Policy LU-2.1:

Plan for a balanced development pattern that includes diversity in employment opportunities, goods and services, residential choices, and open spaces.

Implementation:

The Fullerton Transit Village Specific Plan provides for unique live/work employment opportunities and housing opportunities directly adjacent to public transit.

Policy LU-2.4:

Encourage the maintenance of healthy residential neighborhoods, the stabilization of transitional neighborhoods, and the redevelopment or rehabilitation of deteriorated neighborhoods.

Implementation:

Implementation of the Fullerton Transit Village project would allow for redevelopment of transitional/deteriorated parcels/neighborhood in a manner which significantly enhances the City's downtown.

Policy LU-2.6:

Encourage the development of multiple-family dwellings near employment opportunities, shopping areas, public parks and transit lines.

Implementation:

The Fullerton Transit Village Specific Plan will provide multi-family live/work lofts and townhomes near downtown shopping areas, directly adjacent to public transit.

Policy LU-3.1:

Actively encourage commercial and industrial businesses which contribute to the diversification of an expansion of the City's economic base to locate and/or remain in Fullerton.

Implementation:

The Fullerton Transit Village project is a small example of an emerging trend to locate small businesses in the home, as a diversification of the downtown business community.

Policy LU-4.2:

Balance land use and traffic capacity, so that existing and future development can be supported by the roadway network at traffic levels of service (LOS) no worse than LOS "D".

Implementation:

By capitalizing on higher density residential development opportunities adjacent to regional transit, Fullerton Transit Village can incrementally reduce impacts to the surrounding roadway network, compared to a conventional project.

Goal LU-5.5:

Fullerton Transportation Center: Consider rezoning to guide future "transit oriented" development intensities and land use relationships in proximity of the Transit Center.

Implementation:

Fullerton Transit Village directly implements this goal with higher density residential uses, which can increase transit ridership.

7.2 Circulation

Policy C-2.2:

Encourage the establishment and use of bicycle related facilities and services in public and private developments.

Implementation:

The Fullerton Transit Village Specific Plan is located adjacent to public transit, which accommodates multi-modal bicyclists, and adjacent to city bicycle trails. This encourages the use of bicycles as a form of transportation to work, school and shopping.

Policy C-2.3:

Ensure the provision and maintenance of public sidewalks and walkways where desired in order to facilitate pedestrian mobility and safety.

Implementation:

The Specific Plan land plan provides for dedication of a public access easement allowing new access across the site connecting areas west of Harbor Blvd. to the Transit Station. A pedestrian path from south of the project to the Transit Station will also be provided.

Policy C-3.1:

Encourage and facilitate the use of public transportation and ridesharing for all its residents.

Implementation:

The Fullerton Transit Village Specific Plan encourages the use of public transit with medium density housing and live/work potential directly adjacent to the Transit Station.

7.3 Housing

Policy H-1.1:

Encourage the development of a variety of housing types and costs, for both owners and renters, whereby the widest economic range of housing choice is available.

Implementation:

The Specific Plan would supplement the City's existing housing stock with a unique multi-family program with live/work opportunity. The development will provide a type of housing opportunity fairly unique to the City.

FULLERTON
TRANSIT VILLAGE
PRJ03-00804
APD: 5/18/04
HDR / SPD

SECTION FOUR
Development Standards

- 4.1 General Regulations and Conditions
- 4.2 Permitted Uses
 - 4.2.1 Permitted Uses
 - 4.2.2 Uses Permitted Subject to a Conditional Use Permit
 - 4.2.3 Accessory Permitted Uses
 - 4.2.4 Uses Permitted Subject to a Conditional Use Permit
- 4.3 Residential Developmental Standards
- 4.4 Supplemental Standards
- 4.5 Parking Standards
- 4.6 Accessory Structures and Uses
- 4.7 Signage

7.4 Resource Management

Policy RM-4.1:

Conserve energy, water and other resources.

Implementation:

The Fullerton Transit Village Specific Plan program reduces the reliance on the automobile with high density housing close to transit and shopping, live/work opportunities, and enhanced pedestrian opportunities, conserving fossil fuel resources.

7.5 Community Health and Safety

Policy CHS-1.3:

Encourage physical planning techniques which will help prevent accidents, crimes and fires.

Implementation:

The Specific Plan includes site design regulations with minimized front yard setbacks for multi-family structures, vertical massing, direct access and on-street public parking.

These techniques promote “defensible space”, the public domain, and increased visibility.

7.6 Regional Coordination

Policy RC-2.1:

Participate in regional efforts to improve air quality.

Implementation:

The Specific Plan, when implemented, will incrementally improve air quality with opportunities for public transit, live/work, and enhanced pedestrian mobility.



PLACEHOLDER

APPENDIX

